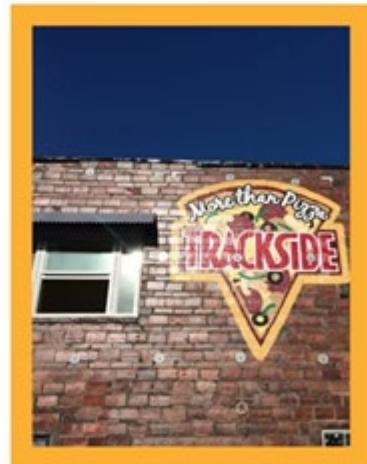




TOWN CENTER PLAN



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I Vision, Goals, and Policies

1.1 Introduction

The Sumner Town Center Plan presents the vision and strategies for Sumner's 197-acre Town Center. The Town Center Planning Area includes Main Street, west Sumner and the Sounder Station vicinity and intends to reinforce and strengthen the Sumner Downtown as a fully functional "everyday" downtown maintaining its classic small-town character. The Sumner Town Center Plan identifies the planning and public process and summarizes and consolidates the various studies developed for the Town Center in one place. This Sumner Town Center Plan is a subarea plan that supports the Sumner Comprehensive Plan. Development and design are regulated through a form-based code adopted into the Zoning Code.

1.2 Purpose

The Town Center is located near the convergence of two freeways, two rail lines, and two rivers (See Figure 1 and Figure 2, Vicinity and Aerial Map, and Parcels and Buildings). The historic downtown, formally the CBD, centered on Main Street, continues to be a focal point of the community with its concentration of businesses and civic uses, and by virtue of its character and history. In 2000, the City and Sound Transit re-established a rail station on the original 1883 train station site, south of Main Street along the Burlington Northern Santa Fe Line (BNSF). In 2018, the boardings at the Sounder rail station had far surpassed the original planning estimates, with over 1,500 occurring per day, rather than the 300 per day forecasted. Unexpectedly, challenges during the pandemic in 2020 economically impacted Downtown, which created opportunities for businesses growth and ingenuity, and ridership at the Sounder Station dramatically lowered. However, that did not interfere with the project plans for revitalizing the Station district with a new Sumner Station parking garage. Commute trip reduction and new policies also affect the pattern of local ridership and traffic regionally. The housing market trends towards increasing costs, making affordable housing development a challenge.

Sound Transit continues to expand their regional rail system with improvements to parking and access, and the Sumner Station parking garage began construction in 2024 after a decade long planning effort.

Following the initial decision to expand Sumner Station and to support downtown businesses, the City engaged in a Town Center planning process in the early 2000s and amended the Comprehensive Plan and adopted new design and development regulations specific to the Town Center plan area. Over the preceding 13 years little development occurred as envisioned.

The Town Center Plan was updated in 2018 with similar goals to the original Plan, which is to encourage development that not only provides housing units near transit options, but also provides additional residents to further enhance Downtown in accordance with the community vision:

- Sumner, a city of excellence reinforcing its role as classic, small town Americana that goes beyond nostalgia, and
- Sumner, a community that retains/strengthens a fully functional, “everyday” downtown.
- Sumner, a community that provides affordable housing for all life stages and recognizes the value of a transit station for convenience of residents.

The Town Center Plan is a roadmap to revitalize downtown as a vibrant gathering place, a stable commercial core, and a neighborhood providing a variety of housing options for Millennials, small families, empty nesters, and other households to live in proximity to the rail station and Downtown businesses.

As a part of the 2025 Town Center Plan update, review includes ongoing monitoring of the Plan and refreshed planning policies consistent with Sumner’s Comprehensive Plan, which further implements and clearly defines direction for the Town Center Plan for the next 10-20 years.

1.3 Summary of Planning Process

The Sumner Comprehensive Plan identifies the need to have a revitalized Downtown as commercial and cultural center supported by residential development and served by multiple transportation modes. The City implements its Comprehensive Plan goals and policies, by its allocation of resources, such as the partnership in the Sounder Train station, and by the formulation of strategies, programs, civic investment such as in roads, parks and infrastructure.

From the inception of the strategy for the Town Center in 2001, development was slow to grow until after the update in 2018 which evaluated changing conditions, including greater than anticipated Sounder ridership, changes in parking demand and patterns, diversification of housing markets and needs, and Downtown economic vitality cycles, the City is revisiting its goals and programs for the Downtown.

Under the 2025 update, the Town Center Plan incorporates policies from the Comprehensive Plan periodic update. To support these changes, the City conducted additional studies and held public forums to evaluate proposed height reductions and zoning district amendments. The City also reviewed plans for improvements to the Main Street corridor and for the activation and design of alleys to accommodate vendors and businesses, as envisioned in the 2021 Main Street Visioning Plan.

A summary of the previous planning process and studies that implemented the 2018 Town Center Plan update is provided in Appendix A. The planning process includes a public participation plan implemented for the 2025 update is provided in Appendix B, containing outreach efforts to affected

businesses and property owners. Recently, the city conducted review on the following studies, forums and plans:

1.3.1 Studies

- **Real Estate and Retail Market Study:** EcoNorthwest, September 2018. This study remains relevant to the 2025 Town Center Plan update. It analyzed the potential for multi-family mid-rise urban style development and potential on specific opportunity sites. Summarized under section 3.3 Market Preferences, opportunity sites are generally vacant or underdeveloped parcels that could have the potential for housing density or commercial uses. Phase 1 opportunity sites suggest potential for change within the next 10 years. Phase 2 opportunity sites are projections of redevelopment 15-20 years from now.
- **Planned Action Ordinance (PAO):** BERK Consulting, September 2025. A PAO is important to the overall desire to facilitate development in the Town Center Plan Area and was amended in 2025 to reflect new thresholds for dwelling units and traffic impacts.
- **2018 Supplemental Environmental Impact Statement:** BERK Consulting. In April 2025, a land use analysis and policy review were conducted for the proposed Town Center Plan updated. The study evaluated assumptions under the 2018 SEIS and a Determination of Non-significance (DNS) was issued on August 20, 2025.
- **Sumner Comprehensive Plan Periodic Update Transportation Study:** Transpo Group. 2024. A comprehensive analysis of the Transportation Management Plan.
- **Town Center Update Transportation Technical Study-** Transpo Group May 2025. A technical analysis of the transportation impacts associated with increased employment targets in the new areas studied in the West Sumner District of the Town Center.

1.3.2 Forums

- **2018 Town Center Plan Update.** A summary of the previous planning process that implemented the 2018 Town Center Plan update is provided in *Appendix A*.
- **Spring Open House, April 2025.** In this public forum staff prepared an illustration for the Town Center Update with changes in building heights and new uses in portions of the Town Center. There was an opportunity for open discussion with planning staff seeking public feedback and comments. The comments and recommendations helped shape the policy and actions for the 2025 Town Center Plan.
- **Field Research, April 2025.** Key staff members toured downtown Auburn to evaluate the build-out of mixed-use development with greater stories and densities. Staff obtained

feedback on street and sidewalk standards, ground-story commercial uses and downtown parking garage development. Developments were up to 6 and 7 stories which allowed for higher densities near transit and building setbacks were greater when the building height was taller. Businesses that are unique for the City of Auburn include medical facilities in the downtown area and few opportunities for artisan/confectionary enterprises

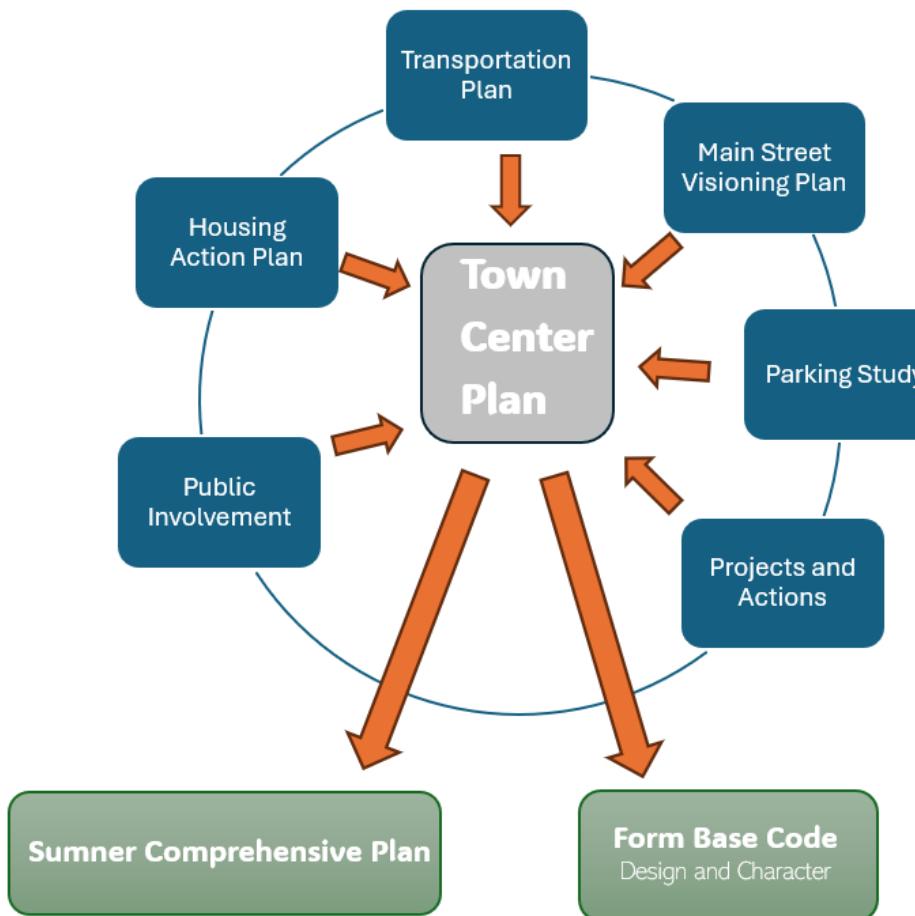
- **Focus Group Meeting, June 2025.** As a part of the public participation plan City staff met with several community members and stakeholders including developers, private property owners, businesses owners, real estate brokers, architects, and civil engineers regarding the proposed Town Center Plan update and IDEA overlay within the West Sumner District

1.3.3 Plans

- **Sumner's Comprehensive Plan, 2024.** The City completed the 10-year periodic update as mandated by the Growth Management Act, adopted by the City Council January 2025. The Plan includes amended policies around Housing, Historic and Cultural resources and incorporates the most up to date plans for Parks and Trails, Capital Facilities, and Transportation. The project and actions matrix under section 1.4 is updated in concert with the 2024 Comprehensive Plan and new actions are included from the Parks, Trails, and Open Space Plan and the 2021 Main Street Visioning Plan, further described below.
- **Parks Trails, & Open Space Plan Update, 2024.** Sumner updates the Parks and Trail plan every six years to assess changes in community needs, track changes in recreational preferences and ensure goals outlined in the City's Comprehensive Plan are met through efforts to maintain, grow and evolve Sumner's parks/trails system. The project and actions matrix under section 1.4 includes updated policies around parks and trails.
- **Sumner Tourism Plan, 2023.** The Sumner Sub-Area Tourism Master Plan approved by Council through Resolution 1671, outlines strategies to provide the best possible experience to visitors and the greatest return possible on the collective investment. Strategic goals include enhancing tourism infrastructure, augmenting connectivity and mobility, and strengthening identity and alignment. The Tourism Plan aligns with the county-wide Strategic Tourism Master Plan. Where applicable, strategies from the plan are incorporated in the Town Center Plan project and actions table, under section 1.4.
- **Main Street Visioning Plan, 2021.** The 2021 Sumner's Main Street Visioning Plan (MSVP) advances the Town Center Plan and is primarily focused on the Historic Commercial Business District. The plan has three primary focus areas Heritage Park and its surrounding streets; design and activation of alleys; and the Main Street corridor. These elements are to be further designed in detail for construction as funds are available for implementation. The elements interact and relate to each other through physical connections, pedestrian corridors, use as event space and design characteristics. The City Council considered action priorities

for implementing the Main Street Visioning Plan and discussed these priorities on July 26, 2021, and adopted Resolution 1605. These goals are integrated into the project and actions matrix under Table 1.

Each of the studies, plans and forums contribute to the Town Center Plan update and are discussed in greater detail below. The Town Center Plan in turn will guide some refinements of the Comprehensive Plan and serve as a master plan that supports the Comprehensive Plan. The diagram below shows the relationship of the studies/forum to this Town Center Plan and to the Comprehensive Plan.



1.4 Projects and Actions

The connection of the Sumner Community to the Sounder Train Station and the connection of Sumner as a whole to the region led to several planning studies starting in 2001, addressing land

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use/market conditions, parking, and other functional concerns in the Sumner Downtown. The City of Sumner City Council desired a visionary and transit-oriented plan to bring residents to Sumner. Therefore, the Town Center planning process was revisited with both a broader vision and more detailed strategies captured in the *December 2002 Sumner, Washington Town Center Strategy, Phase I* report.

The *December 2002 Sumner, Washington Town Center Strategy, Phase I* report remains relevant to Sumner's efforts in strengthening its Downtown. The strategy reinforced and articulated a vision for Sumner as a classic small town that retains and strengthens its center as a fully functional and an “everyday” downtown. The Town Center Strategy recommended eight strategic components to develop a successful Town Center that are still relevant in 2025.

- Make a Great Plan
- Many, Many Projects and Actions
- Many, Many Stakeholders
- Committed Ongoing City and Private Sector Leadership
- Performance Standards
- Communication and Marketing
- Supportive Government
- Ongoing Review

The Town Center Strategy report elaborated upon the first two strategic components – **“making a great plan” and having “many, many projects and actions.”**

Defining a “great plan”, according to the authors:

- Establishes the desire to create a plan that will succeed and promote three pronged growth (economic, sustainable, livable);
- Goes far beyond patching problems with quick-fix solutions or reacting to specific issues;
- Presents a strong-enough vision to motivate and enliven people to take action; and
- Addresses long-term possibilities regardless of short-term constraints.
- Allows for flexibility to promote development, but doesn’t lose the vision of the plan.
- “Many, many projects and actions” refers to maintaining a variety and number of projects and actions, which may include infrastructure improvements, housing, open space, cultural facilities, community music and art festivals, local lending pools for revitalization, plans/regulations, and other categories, to implement the

“great plan” rather than a single big project that would “save the day.” The target number of projects and actions was suggested at 25 to help assure “forward momentum.”

A recommended list of projects and actions was provided in the Town Center Strategy report. The City responded to the report by preparing a “Jump Start Matrix” of projects, responsible parties, and priorities to provide momentum for the “many, many projects and actions” strategy.

- For the 2025 Town Center Plan update, the matrix of projects and actions listed under Table 1 has been adjusted to reflect updated and ongoing project actions.
- Table 2 summarizes completed projects and actions since the adoption of the 2018 Town Center Plan.

Table 1. Projects and Actions – Updated for 2025

PROJECTS AND ACTIONS	PROJECT NUMBER AND DESCRIPTION	PRIORITY
1. Identify, encourage and implement between 5 and 6 housing projects over the next 10-20 years, totaling between 500 and 1,000 dwelling units.	H1: Increase housing stock by 500 to 1,000 units by the year 2044.	High
2. Redevelop Red Apple Market Site	E1: Facilitate the development of the Red Apple Market site as an anchor for extending “Downtown” to Maple Street.	High
3. Develop the Heritage Park block as a public open space that includes a public building with activated gathering space.	H1: Increase housing stock by 500 to 1,000 units by the year 2044.	High
4. Expand support efforts and partnerships with merchant association.	I1: Redesign the Heritage Park stage for better entertainment options and functionality. MSVP1: Create more intentional daily uses including seating, a small playground and a pavilion.	Low

PROJECTS AND ACTIONS	PROJECT NUMBER AND DESCRIPTION	PRIORITY
5. Reinforce the commercial core on Main Street with development at Main Street and Wood Avenue as a gateway into Downtown	I2: Improve Downtown intersections with woonerf paving and Alley activation.	High
6. Maintain public parking behind Main Street including street and business access.	I3: Maintain and improve public parking behind businesses – landscaping, lighting, and signage.	Medium
7. Pursue restaurants, and active retail businesses to open adjacent to public open space.	E3: Recruit businesses to downtown. E4: Recruit small grocery operators like Trader Joe's for Town Center.	High
8. Work with current/incoming retail to expand downtown to include evening options.	E3: Recruit businesses in the downtown.	High
9. Attract a diversity of businesses and promote IDEA uses within the West Sumner District.	E5: Explore business grant incentives. E6: Add new incubator business and co-working spaces within the Town Center IDEA Overlay Zone.	Medium
10. Create opportunities for development of higher density housing in West Sumner.	H3: Create higher density in West Sumner with form-based code and design standards. E7: Incentivize senior housing development.	High
11. Create visual terminus at the end of a street corridor.	I4: This could be a public or private investment in art, architecture or open space feature.	Medium
12. Add more, smaller green spaces as well as more “hardscaped” plazas as development and redevelopment occurs.	I5: Identify and create downtown public gathering spaces.	Low

PROJECTS AND ACTIONS	PROJECT NUMBER AND DESCRIPTION	PRIORITY
13. The City should encourage public or private “third places” – gathering places unrelated to home or work.	E3: Recruit businesses in the downtown.	Medium
14. Leverage the library site to improve connections to the Sumner Links Trail and River with access points and overlooks leading to a trail.	I6: Develop the Sumner Links Trail north of Cannery Way Bridge along the River.	Medium
15. Market the current library site and redevelop the site for senior housing, mixed use or an open space amenity.	I7: Relocate Sumner library to East Main Street.	High
	H1: Increase housing stock by 500 to 1,000 units by the year 2044	
16. Consider reducing/removing impact fees and alternative incentives for desired development.	H3: Create incentives for higher density housing in the Town Center area.	Medium
17. Plan for the eventual redevelopment of the auto dealerships on Traffic Avenue.	H2: Continue communication and partnership for Sunset Chevrolet's continued success in Sumner as the area develops and changes.	Low
18. Acquire key parcels for public parks and gathering places consistent with the Parks, Trail & Open Space Plan.	I8: Acquire private lots for public use.	Low
19. As increase density occurs along Fryar/W. Main, require developers to reconnect community with access/gathering points along White River.	H1: Increase housing stock by 500 to 1,000 units by the year 2044.	Medium
20. Redevelop the Lucy V. Ryan property into a full park to serve as a community asset.	I9: Create a form of artwork/signage to commemorate the Lucy V. Ryan deed.	High

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PROJECTS AND ACTIONS	PROJECT NUMBER AND DESCRIPTION	PRIORITY
21. Other ongoing projects and actions.	I10: Construct downtown amenities.	High
	E7: Conduct a new parking study within 5 years of the 2025 update	High
	E8: Continued partnerships and efforts for successful tourism industry to Sumner and region, consistent with adopted Tourism Plan.	Medium
	E9: Expand holiday lighting.	Medium
	E10: Encourage a mix of retail services in Town Center to keep attractive destination for visitors and local residents.	Medium
	E11: Allow parklets and encourage food truck businesses in the Town Center.	Medium
	E13: Encourage rehabilitation of downtown historic buildings by finding creative solutions for addressing fire and building codes.	Medium

Notes: H = Housing; I= Infrastructure; E = Economic Development; MSVP=Main Street Visioning Plan

Table 2. Completed Projects and Actions

COMPLETED PROJECTS AND ACTIONS	PROJECT NUMBER AND DESCRIPTION	YEAR COMPLETED
1. Acquire the balance of the Heritage Park block to use the entire triangle for public open space.	I1: Expand Heritage Park – acquire remainder of the block for Park expansion. I11: Revise Heritage Park Fountain (removed)	2024
2. Reinforce the commercial core on Main Street with development at Main Street and Wood Avenue as a gateway into Downtown.	I2: Fix up Wood/Main intersection (transportation improvement). (completed)	2024
3. Update freeway signage	E7: Change the sign on the Freeway to add Sumner (currently says East Main/Traffic Avenue).	2020
4. Removed street trees from Main Street sidewalk and replace with planter boxes	I12: Review and update street tree planting and management plan for historic downtown.	2024
5. Adopt a new regulatory approach to create a transit and pedestrian supportive environment and mixed uses (new zoning district, Form-Based Codes, building heights, etc.).	E5: Review permits processes and design and development regulations for impediments to development. E6: Update the Form-Based Code to establish development regulations that will implement the vision of the Town Center Plan.	2018
6. Town Center Plan objective	E10: Complete the SEPA/GMA Planned Action	2018, 2025 Amended
7. Expand the Downtown experience	E11: Finalize Downtown Sound System and expand as needed as Town Center Plan develops	2023

Note: Listed as "Table 1.2" in adopted Ordinance 2939; table numbering corrected for easier navigation

1.5 Public Workshops & Forums

October 2017 – Public Forum

Following preliminary work by staff examining alternatives and housing options, the City held a public forum on October 5, 2017 – Town Center Plan Kickoff Meeting. Participants included citizens and business owners. This public forum presented visual preferences on different multifamily styles and types, streetscapes, and single-family developments. More than 200 attendees provided input on Business Growth and Economics, Housing Development, Parks and Open Space and Transportation. Input received is summarized as follows:

- **Visual Preference Survey:** A series of illustrations were provided showing various housing types, scales, and heights including mixed-use formats and stand-alone multi-family structures. High vote counts tended toward buildings that would fit the character of the historic downtown and utilized brick facades and traditional architecture. Mixed use structures also received very positive response from those attending.
- **Map Coloring Exercise:** Blank maps of the Town Center Plan area were provided with a legend indicating building heights at 2-3 stories, 4-5 stories, and 5-6 stories. In total 24 maps were completed and indicated that those participating could envision increased height in the downtown, particularly close to the Sounder station and along Traffic and Fryar Avenues and further west of Traffic Avenue. The results from this exercise gave clear direction to staff to pursue alternatives that include increasing heights from the current 3-4 story limit to 5-6 stories in areas where it was appropriate.
- **Transportation:** Parking and transportation, such as transit, was often brought forward in conversations and comments received from the public. There were concerns that new development would exacerbate the current parking situation in the downtown. Seniors also expressed a need to have more options for transit.
- **Needed Commercial Uses:** More restaurants with a variety of offerings were also requested.
- **Senior Housing:** A number of senior citizens attended the event and were concerned about the lack of affordable housing in the community for seniors. They requested that the plan consider incentives for affordable senior housing.

1.6 Public Open House

February 2018 - Open House

On February 15, 2018, the Open House was a chance for the public to come and see various alternatives that were prepared based on the public input in October 2017, and staff research and refinements. Staff facilitated small group presentations to explain the two alternatives being presented, the rationale behind them and how they differed. Boards were also presented showing

renderings of street views from various prominent intersections to provide a feel for how the area would look if redeveloped with taller buildings.

- **Attendees:** Thirty-three people attended the Open House and offered comments and input on the plan alternatives. Of the people attending 24 lived in Sumner, 6 worked in Sumner, and 5 were business owners. When asked in a survey where they plan to be in 10 years 15 said they'd be living in Sumner and owning a house, 2 said they would be in multi-family senior housing, 3 said somewhere else, and 9 were not sure.
- **Alternatives:** The voting between Alternatives 1 and 2 were split evenly with 12 votes for Alternative 1 and 13 for Alternative 2.
- **Comment Sheets:** There was a wide range of comments (See Appendix C for completed listing) summarized as follows:
 - ◆ Concern about loss of small town feel, safety and security
 - ◆ Concerns about the potential transient nature of tenants
 - ◆ Transition areas between single-family residential should have lower heights
 - ◆ Take advantage of views of the river
 - ◆ Density and growth around transit is a good idea
 - ◆ Continue to attract younger crowd
 - ◆ Take development slow and re-evaluate often
 - ◆ Provide incentives such as reduced parking and reduced fees to encourage development
 - ◆ Concerns about traffic and school impacts
 - ◆ Prefer townhomes and condos at a smaller scale, 5-6 stories is too much
 - ◆ Care that not too much commercial is built and left vacant

April 2025 - Spring Open House

The City Hall Open House is a great opportunity to reach out to the public and present proposals and alternatives in detail. On April 15, 2025 the Spring Open House kicked off the notice of application period for the Town Center Plan Amendments. Staff prepared illustrations of the Town Center plan updates, including a vision for new uses in portions of the Town Center and reducing story heights in strategic locations. Discussions were held with the public regarding opportunity sites, future pedestrian connections through the Town Center and Form-based code development standards. The following outcomes from the 2025 Open House were:

- **Attendees:** 10 people attended the Open House for comment on the Town Center Plan updates. Of those who participated in public comment, 2 lived in the Town Center boundary, 2 were Sumner business owners and they all resided in the city. From the discussions on the proposed changes, only 2 residents submitted written public comment at the Open House.
- **Alternatives:** There were 2 alternatives presented, a no action alternative that retained existing story heights and existing uses, and the alternative that proposes

a reduction in building heights from 6 stories to 5 or 4 stories, and within the Downtown along Main Street, reducing the story height from 5 stories to 4 stories. There were none that opposed the reduction in story heights, and one commented that they preferred 2 story heights on Main Street.

- **Public Comments:** All public comments received during the notice of application on the 2025 Town Center plan are included in the Appendix D

1.7 Development Community Interviews

2018- Town Center Plan Update

The number one priority of the Town Center Plan is to promote housing in the downtown with a goal of creating between 500 and 1,000 units by 2044. To achieve these numbers and to create the character and streetscape desired in a traditional downtown requires a “form” of construction that utilizes understory parking. This type of “mid-rise urban” development is expensive and potentially challenging in the Sumner market. To help identify housing opportunities and approaches in the Town Center, and to solidify the direction of the Town Center Plan update in 2018 the City interviewed a range of experts in planning, development, architecture, and engineering. Participants included:

- Jerome O’Leary, Real Estate Broker—Kidder Mathews Commercial Real Estate
- Paul Green, P.E., Civil Engineer—Azure Green Consultants
- Bruce McKean, AIA, Principal—Helix Design Group (architects)
- Curt Gimmestad, Director of Operations—Absher Construction, Inc.
- Matt Smith, President—Rush Companies
- Chris DeWald, Vice President of Development—Rush Companies

Key points from the interviews were as follows:

- Sumner is an emerging market and developers agreed that Sumner should be looking to the future for increased transit-oriented housing given the high cost of housing in Seattle and changing mindset around the reliance and use of the automobile.
- Parking and Transportation:
 - Parking ratios for multi-family needed to be on the order of a minimum of 1 parking space per multi-family unit with no maximum. Allow visitor parking to occur on the street.
 - Office and commercial parking will need to remain at current parking ratios and to provide parking for customers and employees.
 - There should not be a maximum cap on parking, especially for commercial and office spaces.
 - The City should plan to budget for a parking study every 5 years and adjust as needed.
 - A city transit option (e.g. “trolley”) that would serve the Town Center and

provide access to the east side of the City and such amenities as grocery stores and the YMCA would be very beneficial to residents in this area.

- Height/Housing Densities:
 - Mid-rise Urban residential is on the increase. (mid-rise urban is typically a style of housing less than 7 stories in height and has ground floor structured parking with housing units constructed over the parking).
 - Rent rates will need to continue to rise in order to meet the \$2.25-\$2.50 threshold for profitability.
- Land Use and Form Based Code:
 - Advised against requiring commercial on the ground floor, but allow developer flexibility.
 - Form-based code was seen as providing additional flexibility if the housing densities and other aspects of the use within the structure were not regulated.
- Incentives for Developers:
 - Flexibility in zoning requirements was seen as positive.
 - The adoption of a Planned Action Ordinance, essentially completing environmental review at the planning stage and not requiring it at the individual project development level would save time and reduce risk for the developer.
 - The City should pursue a change in state law that would allow the City to adopt the multi-family tax exemption program. Multi-family development within the Town Center would be exempt from property taxes for 8-12 years depending on if it was market rate or affordable housing.
 - Reduce traffic impact fees and other fees when possible. Retain or extend the traffic impact fee reduction in the Town Center Plan area.
 - Reduce traffic impact fees and other fees when possible. Retain or extend the traffic impact fee reduction in the Town Center Plan area.
- Concerns and Questions:
 - Concerns about a high ground water table that would make subgrade parking more expensive.
 - Questions about infrastructure capacities such as sewer, water, and stormwater.
 - Traffic and congestion and what the City was doing to help with these issues.

The Land Use and Housing and Implementation Strategies sections of this Town Center Plan incorporate these and other recommendations.

June 2025 - Focus Group Meeting

For the 2025 Town Center Plan update, the City conducted a Focus Group Meeting for property owners, businesses and stakeholders that are located in and within the vicinity of the IDEA overlay in the West Sumner District. On June 10, 2025, the Focus Group Meeting was held at the Sumner Library, near the area of affect. A total of 21 properties are on Zehnder Street and 57th Street. A

summary of the attendees is provided below. City staff presented visual examples of different mixed-use styles and explored IDEA uses and types.

Some of the participants included:

- **Attendees:** Sumner/Puyallup/Tacoma Chamber representatives; Commercial/Industrial Real Estate brokers; Sumner citizens and (1) local business.

Key takeaway points:

- **Maintain the six-story height allowance.** Development from the ground up is costly, and additional density is needed to make projects financially feasible.
- **Encourage shared spaces.** Common areas within the district provide opportunities for scalable enterprises and collaborative activity.
- **Support flexible incubator workspaces.** These spaces can help transition between office, retail, and small-scale manufacturing uses, supporting a range of business needs.
- **Offer a variety of unit sizes.** While incubator spaces can be as small as 300 square feet, participants preferred a range of 600 to 1,000 square feet to accommodate growth.
- **Promote clustering.** Grouping smaller makers and incubator spaces around an anchor use helps attract a younger demographic and creates synergy within the district.
- **Consider incentive-based strategies.** The City should explore incentives to encourage microenterprise and small-scale manufacturing as part of the district's economic development strategy.

1.8 Town Center Plan Vision, Goals, and Policies

This section distills the various study and forum recommendations into the Town Center Plan Vision, Goals, and Policies. These will be incorporated into the Comprehensive Plan and can be used as guiding principles as new developments or capital improvements are proposed. These goals and policies should be evaluated periodically. The City will be the primary agency responsible for implementation, based on a public/private partnership with stakeholders.

1.9 Town Center Vision

The past, present, and future vision for the Town Center is:

- Sumner, a city of excellence reinforcing its role as classic, small town Americana that goes beyond nostalgia, and
- Sumner, a community that retains/strengthens a fully functional, full life cycle, “everyday” downtown.

- Sumner, a community that provides housing for all life stages and recognizes the value of a transit station for convenience of residents.

1.10 Goals and Policies

This section is intended to highlight policy concepts that would further the vision for the Town Center. The goals and policies of this section consist of existing and proposed Comprehensive Plan goals, policies, and objectives. The nature of the text as “existing” or “proposed” and their existing or desired location in the Comprehensive Plan is indicated in parentheses at the end of each policy.

Goal TC-1 Make Downtown a vibrant activity area.

Policy TC 1.1	Establish the Town Center Plan area as the community core encouraging a combination of commercial, civic, cultural, recreational, and residential uses. (Existing, Community Character Element 3.1)
Policy TC 1.2	Recruit restaurants, cafés, microbreweries, public facilities and other similar uses, which serve as gathering spots within the community and encourage evening activity in commercial areas. (Existing, Economic Development Element, Policy 1.10)
Policy TC 1.3	The City should, in partnership with the downtown business association(s) and Chamber of Commerce, to actively recruit and promote pedestrian-oriented retail, service, and mixed uses. Uses that require outdoor storage, provide offsite services, or have little walk-in trade as primary parts of their businesses should be oriented to secondary locations in Downtown away from Main Street and Alder Avenue and designated Activity Nodes. (Existing, Community Character Element, Policy 3.6)
Policy TC 1.4	Allow and encourage activities catering to a range of ages, interests, ethnicities, and other diverse attributes. (Existing, Community Character Element, Policy 3.8)
Policy TC 1.5	Through the use of form-based code, street and design standards, promote pedestrian and bicycle friendly streets with trees and other amenities that enhance the streetscape. (Existing, Community Character Element, Policy 2.5)
Policy TC 1.6	Encourage more housing in the Town Center to strengthen Downtown businesses, take advantage of the commuter train, offer a range of housing in the community, and provide an active, social character the daytime and evening. (Existing, Community Character Element, Policy 3.7)

Policy TC 1.7 Allow dining and other retail activities in the right-of-way where adequate public sidewalk widths are found and explore the potential for seating space in the form of “parklets” in the right-of-way and in alleys where appropriate. (Existing, Community Character Element, Policy 3.8)

Policy TC 1.8 Through the Comprehensive Plan and Zoning Code and form-based code, ensure a central role of the historic central business district, and limit competitive uses as appropriate. (Existing, Community Character Element, Policy 3.4.1)

Policy TC 1.9 Provide for community festivals, outdoor markets, performing arts, or other community events and activities for all ages and backgrounds to promote business and activities in the Town Center, and particularly in the historic downtown core. (Existing, Community Character Element, Policy 3.8)

Goal TC 2 Recruit businesses and activities relevant to everyday lives.

Policy TC 2.1 Promote businesses that meet the daily needs of residents – markets, dry cleaners, laundromats, eateries, pharmacies, banks, pet stores, beauty salons/barbershops, etc. (Existing, Economic Development and Employment Element, Policy 6.14)

Policy TC 2.2 Adopt incentives and actively seek out and recruit developers to construct housing in and near Downtown to strengthen Downtown businesses that will in turn offer goods and services for residents. (Existing, Economic Development and Employment Element Policy 6.5)

Policy TC 2.3 Encourage more mixed commercial/service/housing uses in the Town Center, such as along Traffic and Fryar Avenues. Limit light industrial uses in the Town Center in favor of mixed-use unless there is an educational or artist component. (Amended, Existing Economic Development and Employment Element, Policy 6.15)

Policy TC 2.4 Establish an IDEA Overlay zone in the Town Center along Zehnder Street and 57th Street, to encourage mixed-uses comprised of retail, residential, small-scale production, civic institutions and artisan enterprises. (Amended, new Economic Development and Employment Element, Policy 6.24)

Policy TC 2.5 Give downtown locations highest priority when siting city and government facilities such as libraries and administration offices, which have significant employment or destination potential. (Existing, Economic Development and Employment Policy 6.4)

Policy TC 2.6 Encourage mixed-use areas where small scale commercial and light manufacturing uses can occur consistent with other Comprehensive Plan policies. (Existing, Economic Development and Employment Element, Policy 4.2)

Policy TC 2.7 Implement the actions and strategies outlined in the 2021 Main Street Visioning Plan for the downtown core area. (Existing, Community Character Element, Policy 3.4.4)

Policy TC 2.8 Promote bike trails linking downtown and industrial area. (Existing, Parks and Open Space Element, Policy 3.1.2)

Goal TC 3 Recognize the role of the historic downtown as the historic center of Sumner and heart of the community.

Policy TC 3.1 Ensure the character of the historic downtown is retained as new businesses and buildings are established, such as through design standards, historic preservation ordinances, or other means. (Existing, Historic and Cultural Resources Element, Policy 3.1)

Policy TC 3.2 Enliven and refresh historic downtown by encouraging adaptive reuse of buildings and new construction that acknowledges the historic character of the area. (Existing, Historic and Cultural Resources Element, Policy 3.3)

Policy TC 3.3 Encourage historical societies, historic preservation agencies, community groups, and others to provide access to historic documents, such as maps, photos, and newspapers, for landowners, residents, and others. (Existing, Historic and Cultural Resources Sub-Element, Policy 1.3.2)

Policy TC 3.4 Provide ongoing support and expansion of walking tours that highlight public art, architecture, history, trees, and other icons of the historic downtown. (Existing, Historic and Cultural Resources Element, Policy 3.4)

Policy TC 3.5 Within the Town Center area, promote the core area as the historic “downtown.” This is generally the area one block north and south of Main Street, within the Historic District and Station District boundaries. (Existing, Community Character Element Policy 3.4)

Policy TC 3.6 Encourage the incorporation of street furniture and features along streets in the historic downtown, such as streetlights and benches, that are compatible with the historic character of the area. (Existing, Historic and Cultural Resources Element, Policy 3.2)

Policy TC 3.7 Promote a diversity of uses within the downtown, which support the activity base by providing employment, recreational, residential, and a variety of commercial activities. (Existing, Economic Development and Employment Element, Policy 6.3)

Goal TC 4 Retain and grow businesses for success and avoid vacancies.

Policy TC 4.1 Invest adequate resources in ways which strengthen downtown. Whenever possible, these monies shall be used to leverage additional dollars from public and private sources for improvements. (Existing, Economic Development Element, Policy 6.1)

Objective 4.1.1 Partnering with the Puyallup/Sumner Chamber of Commerce and downtown business associations to work with local lenders to secure sources of capital for existing businesses' building improvements and/or business expansion. (Existing, Economic Development Element, Policy 6.1.1)

Objective 4.1.2 Assist with business expansions and new establishments such as through preapplication review and consultation and permit fast track for businesses that are proven catalysts for economic growth such as:

- Restaurants and eating establishments in the downtown.
- Manufacturing and businesses that provide family-wage jobs.
- Alternative energy that furthers the community's commitment to new technologies.
- Aerospace and other products or services that bring in money from outside the region. (Existing, Economic Development Element, Policy 6.1.2)

Policy TC 4.2 Create a market and tourism strategy to attract new business compatible with long-range plan and zoning intents, and strengthen and increase the variety of commercial shopping and service opportunities for the community. Allocate resources to implement market and tourism strategy. (Amended, Existing, Economic Development and Employment Element, Policy 6.16)

Policy TC 4.3 Coordinate with downtown business association(s) and Chamber of Commerce on education, e.g. business management, business promotion ideas, entrepreneurship, and explore funding opportunities to incentivize incubator businesses. Partner with local agencies and schools on makerspace development. (Amended, Existing, Economic Development and Employment Element, Policy 6.17)

Policy TC 4.4 Work with building owners to encourage viable uses on the second floors of existing buildings by exploring options in the building and fire codes. (Existing, Economic Development and Employment Element, Policy 6.9)

Policy TC 4.5 Work with downtown stakeholders and the Sumner Downtown Promotion Association to develop and implement a detailed downtown "Main Street"-based revitalization strategy addressing organization, design, economic restructuring and promotion. (Existing, Economic Development and Employment Element, Policy 6.10)

Policy TC 4.6 Create a mixed economy Downtown that provides basic and specialty retail, services, destination attractions, hospitality and entertainment uses. (Existing, Economic Development and Employment Element, Policy 6.18)

Policy TC 4.7 Accommodate mixed use developments with commercial on the ground floor. Require that mixed use buildings within the Town Center and that have the potential for businesses on ground floor install ceiling heights and infrastructure for future conversion of residential to commercial spaces. (Existing, Economic Development and Employment Element, Policy 6.19)

Goal TC-5 Create an enjoyable and identifiable Downtown atmosphere.

Policy TC 5.1 Provide for community festivals, outdoor markets, performing arts, or other community events for all ages to promote business and activities in the Downtown. (Existing, Community Character Element, Policy 3.9)

Policy TC 5.2 Ensure that the Town Center is a clean, safe, and convenient neighborhood, and that it maintains a high quality and attractive appearance. (Existing, Community Character Element, Policy 3.10)

Policy TC 5.3 Enhance "wayfinding" to and throughout Downtown. Have signs on the State routes, at gateways into Downtown, landmarks, public spaces, public parking and other areas. (Existing, Community Character Element, Policy 3.11)

Policy TC 5.4 Ensure that adequate off-street parking is provided for multi-family development that is reflective of the transit-oriented nature of the Town Center Plan area without requiring expensive construction of parking that exceeds demand. (Existing, Community Character Element, Policy 3.20)

Policy TC 5.5 Every 5 years following adoption of the Town Center Plan conduct a study reviewing parking utilization and parking issues and make policy adjustments as needed. (Existing, Community Character Element, Policy 3.21)

Policy TC 5.6 Improve existing public parking lots with signage, landscaping, lighting, and drainage. (Existing, Community Character Element, Policy 3.12)

Policy TC 5.7 Continue to provide and fund maintenance of Public Restroom facilities for the historic Downtown Business District. (Existing, Community Character Element, Policy 6.12)

Policy TC 5.8 Maintain a high quality and attractive appearance. Work with downtown business association(s), Chamber of Commerce and others to seasonally decorate Downtown with decorations, hanging baskets, lights, etc. (Existing, Community Character Element, Policy 3.4.2)

Policy TC 5.9 Prohibit franchise architecture within the Town Center and regulate design through form-based code and design and development guidelines. (Existing, Community Character Element, Policy 3.14)

Policy TC 5.10 Discourage, and in some cases prohibit, drive-thru businesses in the Town Center to encourage pedestrian oriented design and uses. (Existing, Community Character Element, Policy 3.15)

Policy TC 5.11 Through the use of form-based code require new development within the Town Center to provide pedestrian amenities such as landscaping, plazas and fountains and buffers where appropriate. (Existing, Community Character Element, Policy 3.16)

Goal TC-6 Provide for a variety of housing types and densities in the Town Center Plan area in close proximity to the train station.

Policy TC 6.1 Promote the construction of housing stock in the Town Center by at least 500-1000 dwelling units by 2044. (Existing, Housing Element, Policy 4.1)

Policy TC 6.2 Encourage land assembly allowing for feasible and attractive housing or mixed housing/commercial developments. Waive permit fees associated with lot consolidation such as lot line adjustments. Facilitate matching compatible owners that can work jointly to consolidate and sell/develop. (Existing, Housing Element, Policy 4.5)

Policy TC 6.3 Promote and market the redevelopment potential of key downtown through partnering with property owners, developers, Sound Transit,

and other key parties. (Existing, Economic Development and Employment Element, 6.11)

Policy TC 6.4 Adopt greater height limit, 5-6 stories, for development at higher density in West Sumner and west of the White River to promote re-development of the area for transit-oriented development. (Existing, Housing Element Policy 4.2)

Policy TC 6.5 Adopt greater height limits, 4-5 stories, east of the BNSF Railroad with a transition area to single-family residential at a maximum of 3 stories as appropriate. (Existing, Housing Element, Policy 4.3)

Policy TC 6.6 Subject to a form-based code and design review, encourage infill residential development in the Downtown, such as “mixed use buildings” (multifamily units above ground-floor commercial), and “mixed use developments” (combination mixed use buildings and separate residential buildings on the same parcel), in order to allow for an active community core. (Existing, Community Character Element, Policy 3.5)

Policy TC 6.7 Maintain the Town Center “transition areas” that will be at reduced height (maximum 3 stories) and prohibit certain commercial uses, to create a more compatible transition between low density residential zones and areas with increased height. (Amended, Existing, Housing Element, policy 4.4)

Policy TC 6.8 Reduce reliance on automobiles by promoting higher density and infill developments that are located near major transportation links such as the Sumner Commuter Transit Station, and by supporting a network of transit stops connecting neighborhoods to the station. (Existing, Housing Element, Policy 3.2)

Policy TC 6.9 Adopt a “form-based code” in the Town Center to ensure that the design standards for multi-family and mixed-use development are specific enough to create high quality designs and were appropriate be compatible with the historic architectural context, and scale of the Main Street. (Existing, Community Character Element, Policy 3.17)

Policy TC 6.10 Provide incentives for developing senior housing in the downtown such as permit fee waivers and reductions, parking requirement reductions, and multi-family tax exemptions. (Existing, Housing Element Policy 2.2)

Policy TC 6.11 Development along Traffic Avenue should be in a range of 4-6 stories and be of a mid-rise urban style and provide for underbuilding parking. (Existing, Community Character Element, Policy 3.18)

Policy TC 6.12	The height in the Historic Subdistrict should be 3-4 stories to protect and preserve the small-town character and scale of Main Street and reduce conflicts with adjacent residential uses. (Existing, Community Character Element, Policy 3.19)
Policy TC 6.13	Prohibit Garden style apartments in the Town Center that would promote surface parking. (Existing, Housing Element, Policy 4.6)
Policy TC 6.14	In order to promote redevelopment to desired densities, prohibit new single-family residential development and limit expansion except for the addition of accessory dwelling units. (Amended, Existing, Housing Element, Policy 4.7)
Goal TC-7	Provide services and infrastructure to support the Town Center district and Sumner community.
Policy TC 7.1	Reduce the parking requirement for multi-family developments within the Town Center to facilitate development that accommodates the transit rider, anticipates lower car ownership in this area because of transit, and changing trends and services related to transit and transportation such as on-demand services and autonomous vehicles. (Existing, Economic Development and Employment Element, Policy 6.20)
Policy TC 7.2	Within the Town Center allow for visitor parking for multi-family and portions of commercial parking to be counted toward parking capacity for such developments. Smaller retail, restaurants, and other desirable pedestrian-oriented businesses should be allowed without an off-street parking requirement provided they agree to not protest the formation of a future local improvement district for the construction of public parking in the Town Center. (Existing, Economic Development and Employment Element, Policy 6.21)
Policy TC 7.3	Every 5 years, in conjunction with interested parties, examine needs for downtown parking within the Town Center and develop methods for providing attractive, safe, accessible, effective and well utilized parking. Modify and adjust on and off-street parking which serves retailers, other businesses, residents, and major employers downtown shall be implemented based on the 5-year parking study. (Existing, Economic Development and Employment, Policy 6.2)
Objective 7.3.1	Implement short-term, medium-term, and long-term parking strategies as indicated by the 5-year parking study. (Existing, Economic Development and Employment, Objective 6.2.3)

Objective 7.3.2 Redesign and install new parking lot signs that are strategically placed throughout the downtown area. Ensure designation of short and long-term parking, and enforcement of parking limitations. (Existing, Economic Development and Employment, Objective Policy 6.2.1)

Objective 7.3.4 Improve safety by creating additional lighting and pedestrian facilities in parking areas. (Existing, Economic Development and Employment, Objective 6.2.2)

Objective 7.3.5 Encourage businesses conducive to shared parking, or uses that provide onsite, under-building parking. (Existing, Economic Development and Employment, Objective 6.2.4)

Policy TC 7.4 Incentivize transit oriented and higher density land uses that encourage walking or biking near transit stations. (Existing, Transportation Element Policy 8.7)

Policy TC 7.5 Require pedestrian and bicycle-oriented amenities and facilities at neighborhood centers and civic and community uses. (Existing, Community Character Element, Policy 4.2)

Policy TC 7.6 Enhance circulation in the Town Center by implementing the Town Center Plan Street prototypes and cross-sections. (Existing Community Character Element, Policy 7.8)

Policy TC 7.7 Support a shuttle or autonomous transit that would connect the Historic Downtown area with eastern areas of the city. (Existing, Transportation Element, Policy 7.7)

Policy TC 7.8 Encourage tree planting in the Town Center along sidewalks and in public open spaces. (Existing, Community Character Element, Policy 5.6)

Policy TC 7.9 Maintain Reuben A. Knoblauch Heritage Park as a key open space in the downtown and support activities and events that complement downtown businesses and residences (Existing, Community Character Element, Policy 5.8)

Policy TC 7.10 Acquire and improve small parcels of land for developing parks and open spaces in the Town Center. (Existing, Community Character Element, Policy 5.9)

Policy TC 7.11 Identify locations for regional surface water facilities in areas of anticipated redevelopment in Downtown. Promote the establishment of regional surface water management facilities to support infill development and preclude the need for individual on-site ponds and facilities, provide development incentives, encourage efficient use of

	land, and reduce overall facility maintenance costs. (Existing, Capital Facility Element, Policy 1.7.7)
Policy TC 7.12	Pursue creative solutions to accommodate parking for development through the creation of local improvement districts and other means. (Existing, Economic Development and Employment Element, Policy 6.22)
Policy TC 7.13	In the Town Center structured and in-building or underbuilding parking is preferred over surface parking. Limit the development of surface parking. (Existing, Economic Development and Employment Element, Policy 6.23)
Goal TC-8	Expand the City's role to be a proactive facilitator of the Town Center.
Policy TC-8.1	Maintain and enhance a partnership with stakeholders Downtown including downtown associations transit providers, business owners, property owners, residents, and others. (Existing, Economic Development and Employment Element, Policy 1.11)
Policy TC-8.2	Provide and facilitate committed ongoing communication and collaboration between City and private sector leadership toward the goal of business development. (Existing Economic Development and Employment Element, Policy 1.12)
Policy TC-8.3	Keep the momentum for Downtown revitalization through implementation of many economic development, infrastructure and housing initiatives identified in the “Projects and Actions” section in the Town Center Plan. Provide for strategies civic investment and an ongoing partnership with stakeholders to implement the various initiatives. (Existing, Economic Development and Employment Element, Policy 1.16)
Policy TC-8.4	Develop and sponsor a City-wide comprehensive and strategic economic development and marketing plan to implement the applicable sections of this plan. (Existing, Economic Development and Employment Element, Policy 1.8)
Policy TC-8.5	Monitor the success of programs and projects based on common performance measures and adjust as needed. (Existing, Economic Development and Employment Element, Policy 1.13)
Policy TC-8.6	Periodically evaluate and review Town Center planning efforts. Adjust implementing methods to changing circumstances while using the Town Center vision as a continuing guidepost. (Existing, Economic Development and Employment Element, Policy 1.14)

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Policy TC-8.7 Maintain the Multi-family Tax Exemption allowance and create other substantial incentives for re-development in the Town Center by providing property tax relief for qualified affordable housing developments for 12 years. (Existing, Housing, Policy 4.8)

The goals and policies will be implemented by a range of plans, programs, and projects identified.

2 Planning Area Conditions

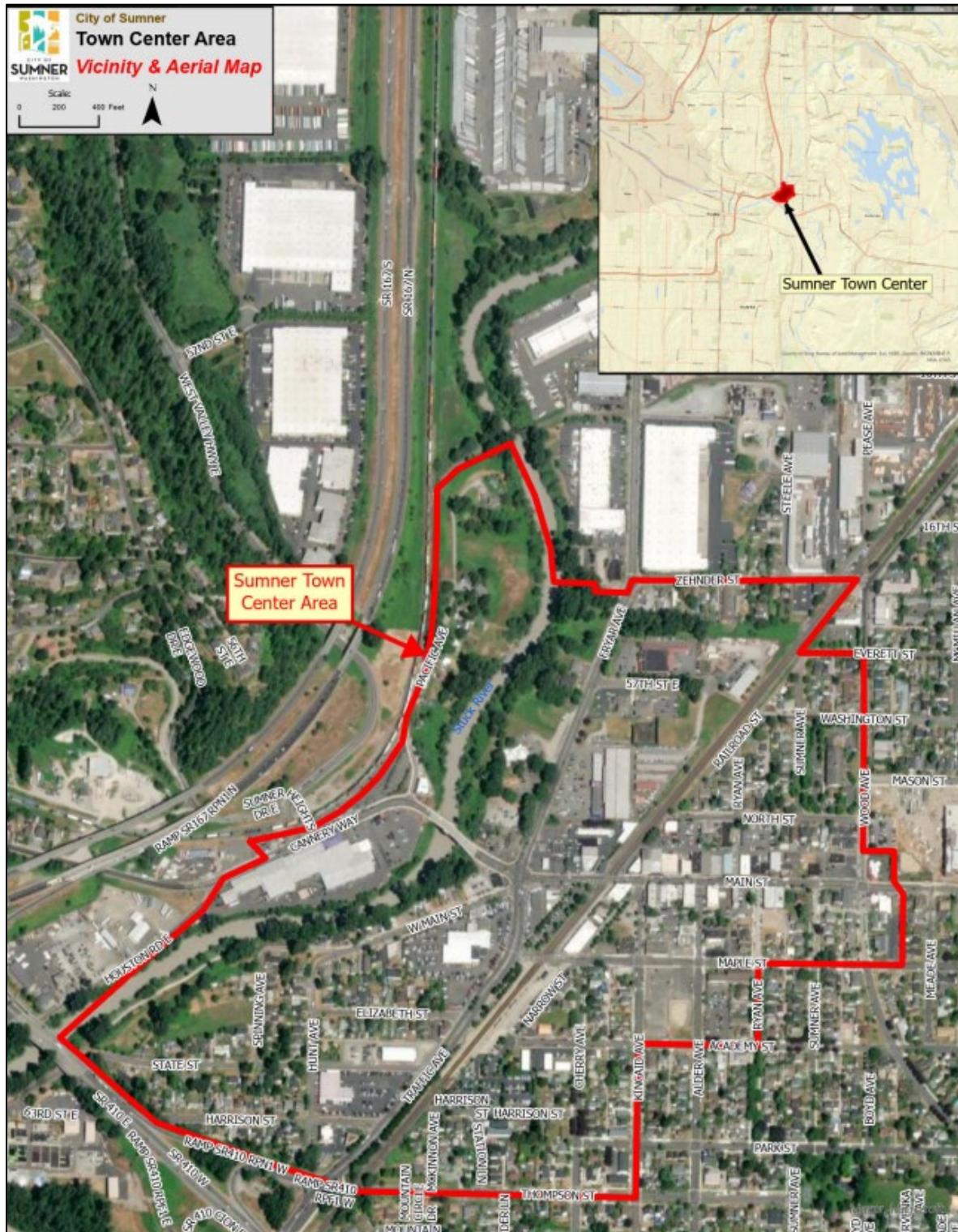
2.1 Town Center Planning Boundaries

The Town Center Planning Boundaries lie within the original town limits established at Sumner's 1891 incorporation, and it encompasses the heart of the Downtown. The area contains the traditional Main Street core, civic facilities, and a growing mix of commercial, residential and institutional uses that have shaped the city's identity for more than a century. Development patterns vary across the Plan Area with a denser more walkable block pattern east of the BNSF railroad, and a more suburban, auto-oriented pattern along Traffic/Fryar Avenue (see Figure 1 and Figure 2).

The boundaries of the Planning Area were drawn to reflect proximity to the Sounder commuter train station, physical boundaries such as major regional and local roadways, and the distinct neighborhood character present at the edges of downtown. Prior study recommendations and community planning efforts identified the Town Center districts as Sumner's highest opportunity for transit-oriented development and economic growth.

The Town Center Planning Area encompasses approximately 197 acres making it large enough to accommodate a range of housing, employment, and civic uses, yet compact enough to retain the small-town scale and pedestrian orientation that defines Sumner's character.

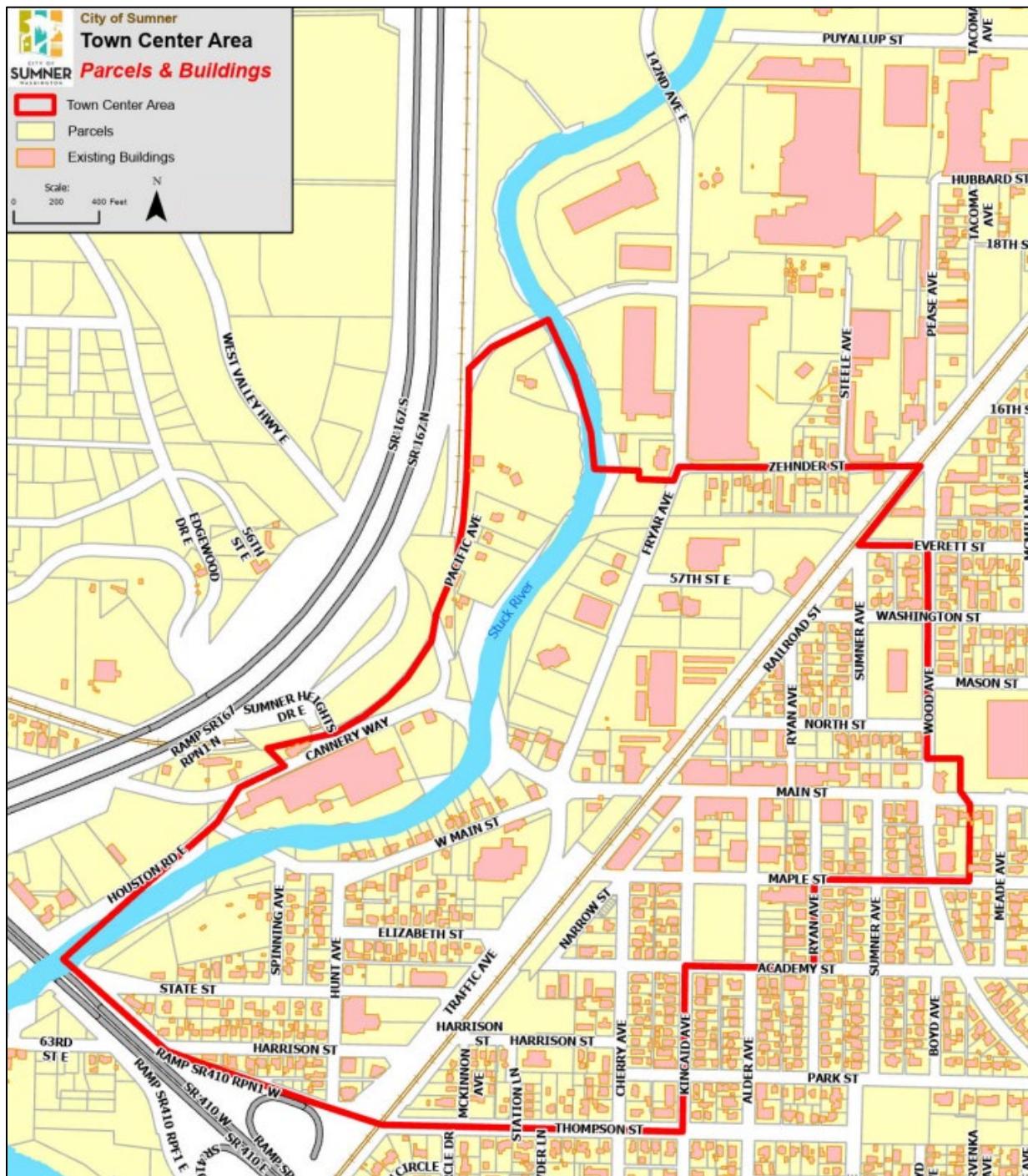
Figure I. Town Center Area, Vicinity & Aerial Map



Source: City of Sumner GIS, August 2025

Note: Listed as "Figure 11" in adopted Ordinance 2939, figure titling corrected for easier navigation

Figure 2. Town Center Area, Parcels & Buildings



Source: City of Sumner GIS, August 2025

Note: Listed as "Figure 12" in adopted Ordinance 2939; figure titling corrected for easier navigation

2.2 Demographics

The Plan Area total population equals 601 persons as of the year 2017 residing in approximately 276 dwellings as shown in Table 3. This represents 6 percent of Sumner's total year 2020 population of 10,621, and is just less 6.7 percent of Sumner's total year 2016 dwellings of 4,125. The Plan Area average household size of 2.36 is smaller than the citywide average of 2.97 according to US Census 2016 data.

The 2016 Census data shows that, all but 21 units in the Plan Area were occupied, and the majority of units were owner-occupied (51.8 percent). This is slightly higher than the citywide ownership rate of 47.0 percent in the year 2016.

Single-family and multi-family breakdowns are unavailable at the Census block level; however, the Pierce County Assessor records indicate approximately 80 multifamily units, which is 22 percent of the units in the Plan Area. A majority of the Plan Area dwellings are single-family at 78 percent. The citywide single-family dwelling percentage is about 53 percent according to US Census 2016 data.

Table 3. Town Center Planning Area Demographics

	Total Population: Total	Average Household Size	Housing Units: Total (2023, With Pipeline)	Occupied Housing Units: Owner Occupied	Occupied Housing Units: Renter Occupied	Total Population: Median Age	Total Population: Median Household Income
Totals/Average	779	2.44	319	143	112	37.9	\$63,563

Source: US Census 2017

Note: Listed as "Table 2" in adopted Ordinance 2939; table numbering corrected for easier navigation

The Plan Area has a household median income of \$63,563 which is higher than the city as a whole with a median income of \$56,000. The population is fairly homogenous in terms of race and ethnicity with 87 percent of the Plan area population identifying themselves as white in the 2016 Census. Eleven percent of the Plan area population identified themselves as Hispanic or Latino. This is similar to the racial and ethnic makeup of the community as a whole which is 83 percent white. Nine percent of the citywide population identified themselves as Hispanic or Latino.

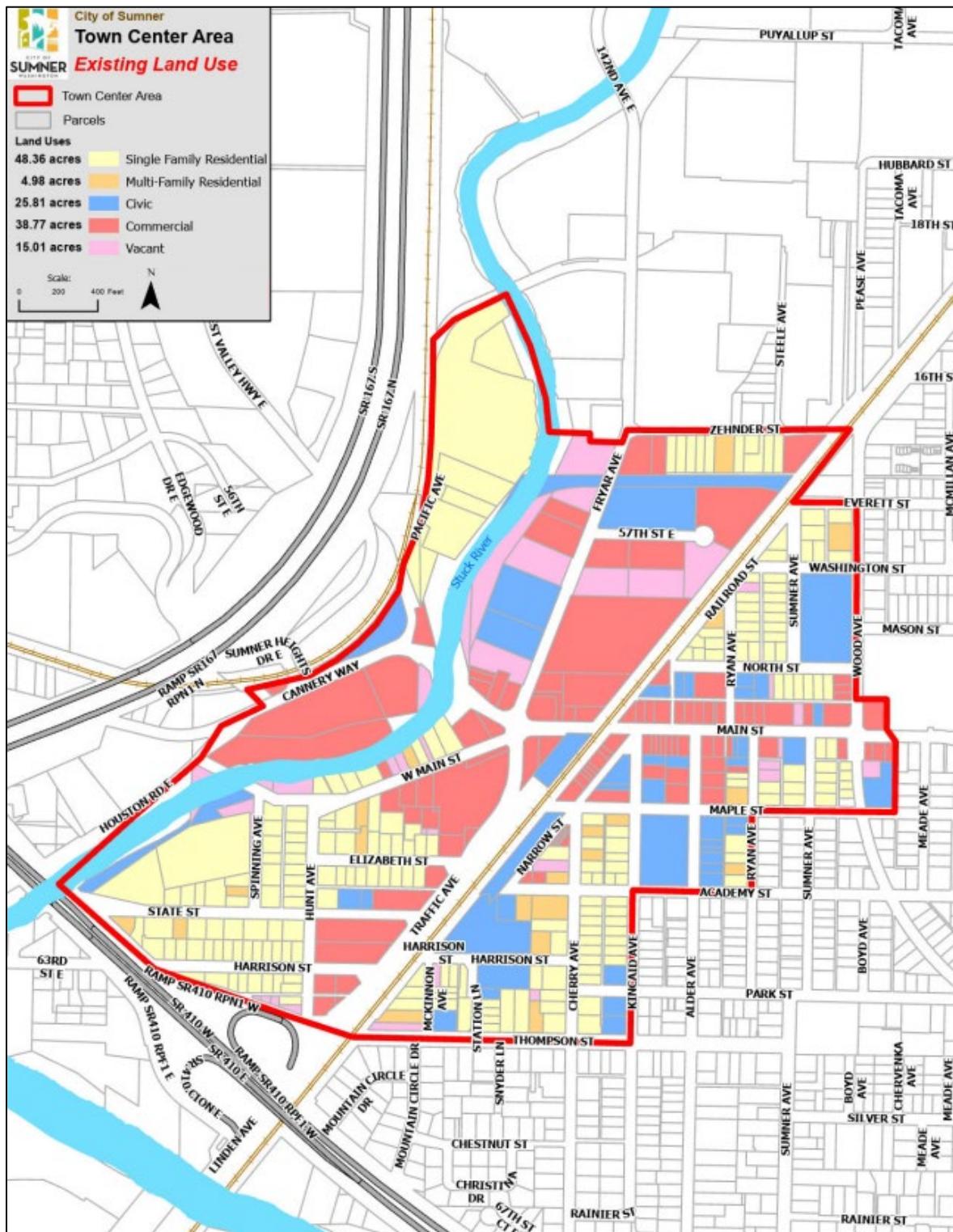
Median age across Plan Area is 37.9 years and according to 2016 Census data 36.3 years city-wide.

2.3 Existing Land Uses

The Town Center Plan area contains a variety of land uses with concentrations of commercial uses along Main Street and Traffic/Fryar Avenue. Residential dwellings tend to be single family and surround the commercial uses east and west of Traffic Avenue and north and south of Main Street.

The greatest acreage is devoted to single-family residential uses, although commercial and industrial acreage is substantial. Civic uses, which include City, School District, Federal, Railroad, and Religious properties are scattered in the Plan Area. The amount of land devoted to multifamily uses is relatively small. Figure 3 illustrates existing land use in the Town Center. Table 4 summarizes existing land use categories and acres. This information relates to uses “on the ground.” The potential for other uses in the Plan area is governed by the Comprehensive Plan and Zoning regulations (described in Section 4).

Figure 3. Existing Land Use Map



Source: City of Sumner GIS, August 2025

Note: Listed as "Figure 13" in adopted Ordinance 2939; figure numbering corrected for easier navigation

Table 4. 2025 Existing Land Uses and Acres

EXISTING LAND USE CATEGORY	NET ACRES	PERCENT
Civic (public, transportation, and religious properties)	18.78	14%
Commercial	34.95	25%
Industrial	3.82	3%
Multi-Family Residential	4.98	4%
Single Family Residential	54.17	39%
Civic	7.03	5%
Vacant	15.01	11%
Total	138.74	100%

Source: City of Sumner GIS, August 2025

Note: Numbers and percentages are rounded. Acres reported are net and include parcel/property area. If considering roads and river area, the total Town Center Plan Area equals 197 gross acres approximately.

Note: Listed as "Table 3" in adopted Ordinance 2939; table numbering corrected for easier navigation

3 Housing

Housing creates community character, sense of place, as well as provides for basic shelter. A range of dwellings allows households to meet their varied needs and incomes over lifetimes, such as students, starter families, established families, “empty nesters,” retired persons, etc. Sumner offers a variety of detached, attached, work-live, assisted, and other types of units at appropriate locations in the City. A unique opportunity exists in the Sumner Town Center to provide for greater number of dwellings in proximity to transit and the City’s commercial and industrial employment centers. Greater housing in the Sumner Town Center would help provide more customers for Downtown businesses and result in a more active, lively center. Attracting more housing was the number one priority of the City Council for the Town Center.

3.1 Housing Types and Densities

Table 5 shows sample dwelling densities, potential configurations, and sample photos illustrating several housing types, including two units, residential attached, multi-unit with over 5 units, and mixed use with apartments over commercial. Each housing type has different implications for how much land is required, building height, appearance from the street, open space, etc.

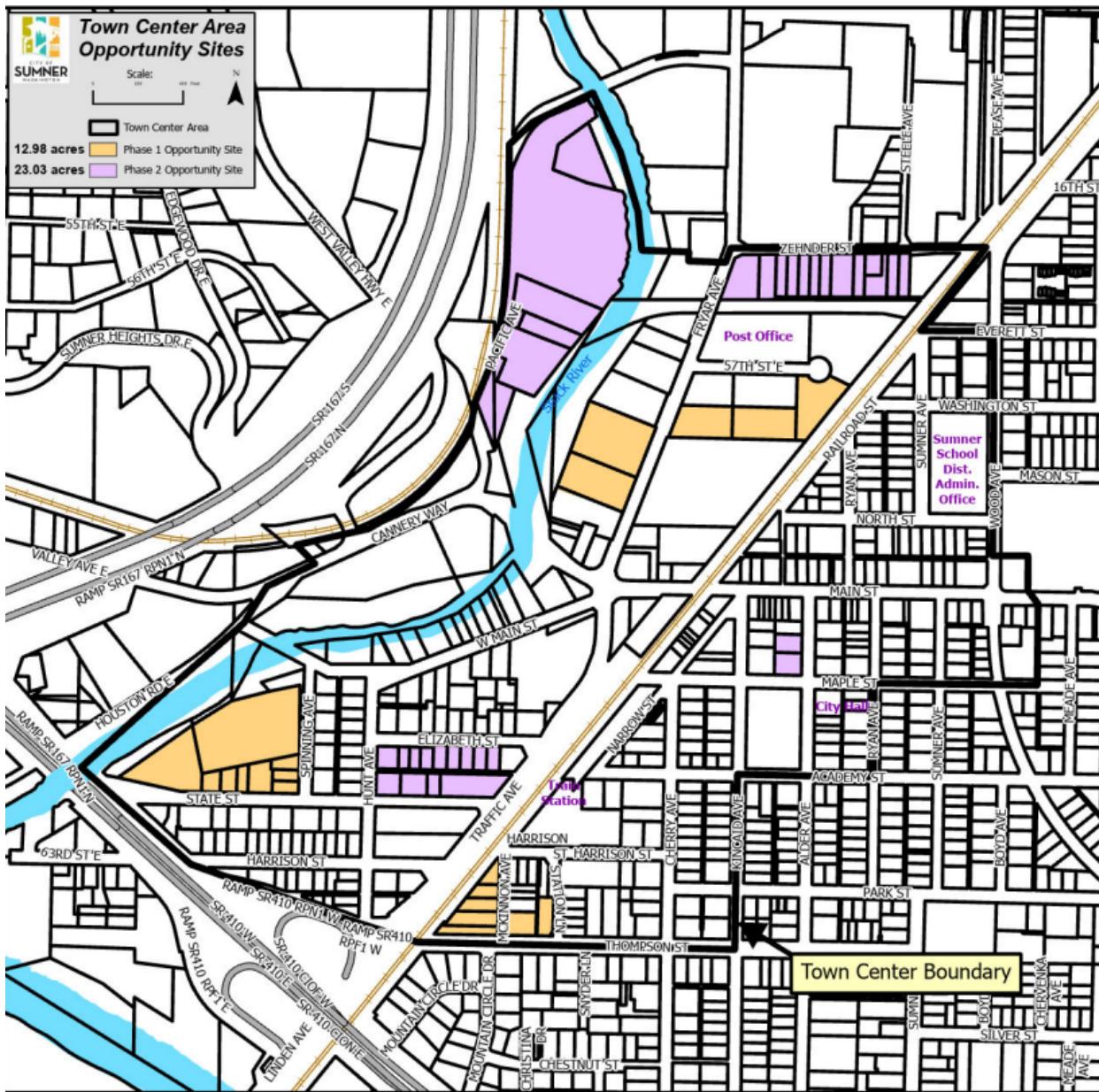
3.2 Design Standards

The form-based code will indicate which housing types are appropriate for the different districts within the Town Center Plan area. Density will not be specifically regulated, rather the height, setbacks, open space and parking requirements will dictate the size of the building and the development will not be limited by density. However, projects with densities that exceed the planned densities analyzed in the Supplemental EIS and covered by the Planned Action Ordinance would likely be required to provide further transportation analysis and potential mitigation.

3.3 Market Preferences

The Plan Area contains vacant, underdeveloped, and redevelopable parcels that could be developed with housing or mixed uses under the proposed zoning. Figure 4 identifies potential development sites.

Figure 4. Opportunity Site Analysis Map



Source: Sumner GIS, August 2025

Note: Listed as "Figure 14" in adopted Ordinance 2939, figure numbering corrected for easier navigation

Phase 1 Opportunity Sites include:

- Sumner Library Site: The City of Sumner and the Pierce County Library District are actively seeking to move the Sumner Library from the current location to East Main Street. This would free up about 3 acres. A 1-acre property is currently under construction with 123 units. This site has the potential for similar densities.

- Spinning Avenue Site: This 5.28 acre area contains approximately 3 acres that is vacant. This is the only large residentially zoned parcel in the West Sumner District and because of that has high potential for redevelopment. To reach the full potential several single-family parcels would have to be purchased and assembled. This site has the potential for 112 units per acre.
- 57th Street: This site contains vacant lots 2.81 acres in size and falls within the IDEA overlay for future mixed-use commercial and/or small-scale-manufacturing uses. With more opportunities for increased employment and innovative commercial development, the site would support up to 76 units per acre.
- Thompson Street: This site is approximately 0.56 acres located on Thompson Street and has the opportunity for multi-family development. There are several other parcels approximately 0.657 acres in total on McKinnon for opportunity for redevelopment. In this vicinity of the Station District, the site would support up to 90 units per acre.

Phase 2 Opportunity Sites are those that require assembling numerous parcels which is inherently expensive and time consuming. Given these impediments, the sites are seen as having more long-term potential after most of the Phase I sites have been developed and would redevelop 10-20 years out.

The Phase II Opportunity Sites include:

- Pacific Avenue: This site remains an opportunity site for new housing density and is considered for Phase 2. This is a 12.61-acre site that is adjacent to the White River and contains a 200-foot setback from the river. The area would likely develop as residential with little or no commercial components given the lack of streets and visibility. The area would support up to 112 units per acre.
- Zehnder Street: The north side of Zehnder Street is zoned M-2, Heavy Industrial and the south side has historically been zoned commercial though the area has seen little redevelopment of the area to commercial save for the converted house on the corner of Fryar and Zehnder and the drive-through coffee stand. This 3.73-acre site could develop in a combination residential and commercial, though truck traffic and noise from industrial activities would have to be mitigated. Under the 2025 TCP update, an IDEA overlay has been applied to this area for opportunity for new innovative uses.
- Elizabeth/State Street: This area is located between Traffic Avenue and Hunt Avenue, it includes 3 large parcels associated with the old animal feed barn approximately 1.08 acres in total size. There is approximately 1.307 acres of current single-family residential lots that would require considerable assemblage for redevelopment. This opportunity site would support up to 90 units per acre and include commercial development opportunities.

- Alder Avenue-Nicolson's Site: This is a new opportunity site located in the Station District. The site contains paved parking lots with existing single-story commercial uses on 0.50-acre lot. If the current owners sell or redevelop, future opportunities include up to 4-story mixed use development and allow up to 90 dwelling units per acre. This site is adjacent to the new city activated Hops Alley project. With future development on the Red Apple site, this site becomes an opportunity to extend downtown.

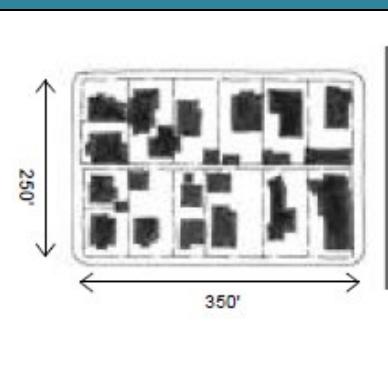
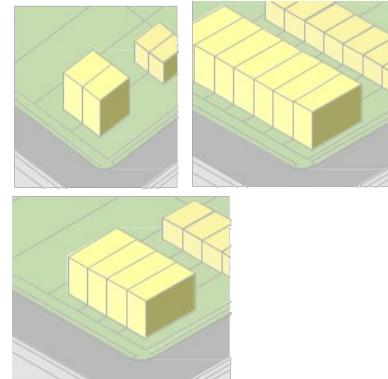
Previous opportunity sites and/or newly developed sites that have completed construction are summarized below:

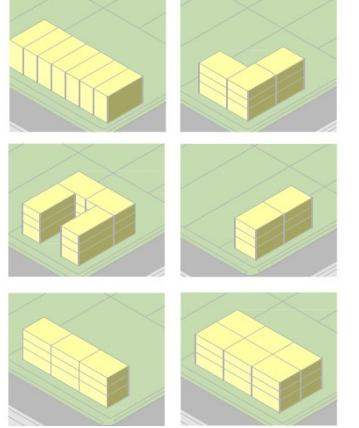
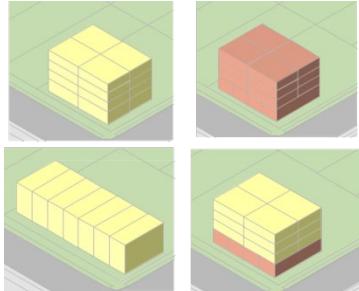
Completed Opportunity Sites:

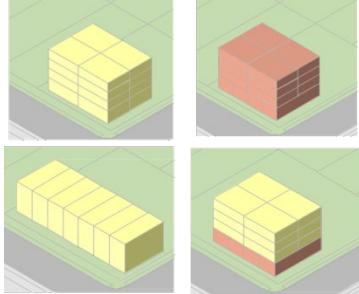
- Red Apple Market Site: Formerly a Red Apple Market grocery store, the city sold this lot in 2020 to a developer who proposes construction of a 3 to 4 story mixed-use building, that would take up the entire 2-acre block of Kincaid, Academy and Maple. The site could accommodate 180 residential units, and the project proposes 4,600 SF of commercial space and 238 on-site parking stalls, with landscaping, outdoor seating and frontage improvements. On the southernmost parcels on the block, existing office buildings have been demolished, the site will undergo soil remediation and redevelopment is likely to occur at the end of 2026.
- Riverfront Development- This development is a new six-story mixed-use building on a 1.091-acre site located in the River District. The project includes 123 residential units, which results in a density of about 117 dwelling units per acre. While the primary focus is residential, the ground floor will feature approximately 989 square feet of commercial space, creating an opportunity for small-scale retail or service uses. The building is 6 stories and will include 164 onsite parking stalls located within a structured parking garage. The amount of parking provided is intended to meet the needs of residents while also supporting the limited commercial space. This development is an example of how the Town Center Plan envisions new housing and commercial uses working together to create a more vibrant, mixed-use district. By adding new housing options in a mid-rise format and providing active commercial frontage at the street level, the project helps promote a lively, pedestrian-friendly neighborhood while making efficient use of a smaller urban lot.

The Form-Based Code is necessary to allow for the proposed scale of development and provide certainty that the design, streetscape and sense of place-making is in keeping with Sumner's desired character and additional housing. On any housing opportunity site, a key concern is ensuring compatibility of new housing with the surrounding neighborhood character through thoughtful planning and design.

Table 5. Housing Types and Densities

HOUSING TYPE	GENERAL CHARACTERISTICS	DENSITY (DU/ACRE) TYPICAL	CONFIGURATION OPTIONS	SAMPLE ILLUSTRATIONS
Existing Conditions: Typical Residential Block in Downtown Sumner	Primarily small lot single-family with some accessory dwelling units or small multi-plex. Avg. lot size =6000 SF	6-13 du/acre (typical); up to 21 du/acre possible		
Single-unit Attached: Townhouse and/or Rowhouse style	2-3 story attached dwelling units, typically grouped in 4-, 6-, or 8-dwelling groups. Front door faces the street with porch or stoop. Unit sizes = 1600-1800sf. 2 parking stalls-1 garage/1 tandem	16-23 du/acre		

HOUSING TYPE	GENERAL CHARACTERISTICS	DENSITY (DU/ACRE) TYPICAL	CONFIGURATION OPTIONS	SAMPLE ILLUSTRATIONS
High Density Residential: Mid-Rise Urban Apartments	Apartment buildings constructed “podium style” with 4-5 stories over 1-2 stories of parking.	80-112 du/acre		
Mixed Use: Mid-rise Urban Over Commercial	Apartment buildings constructed “podium style” with 4-5 stories over 1-2 stories of parking and commercial uses.	80-112 du/acre		

HOUSING TYPE	GENERAL CHARACTERISTICS	DENSITY (DU/ACRE) TYPICAL	CONFIGURATION OPTIONS	SAMPLE ILLUSTRATIONS
Mixed Use: Artisan Lofts and/or Live-Work units	<p>Located in buildings with a mix of residential and commercial spaces. Lofts are known for their open floor plans with minimal interior walls, maximizing space and creating opportunity for live-work.</p> <p>Can be single-unit attached or podium style, with 3 to 5 stories</p>	76-90 du/acre		

Note: Listed as "Table 4" in adopted Ordinance 2939, table numbering corrected for easier navigation

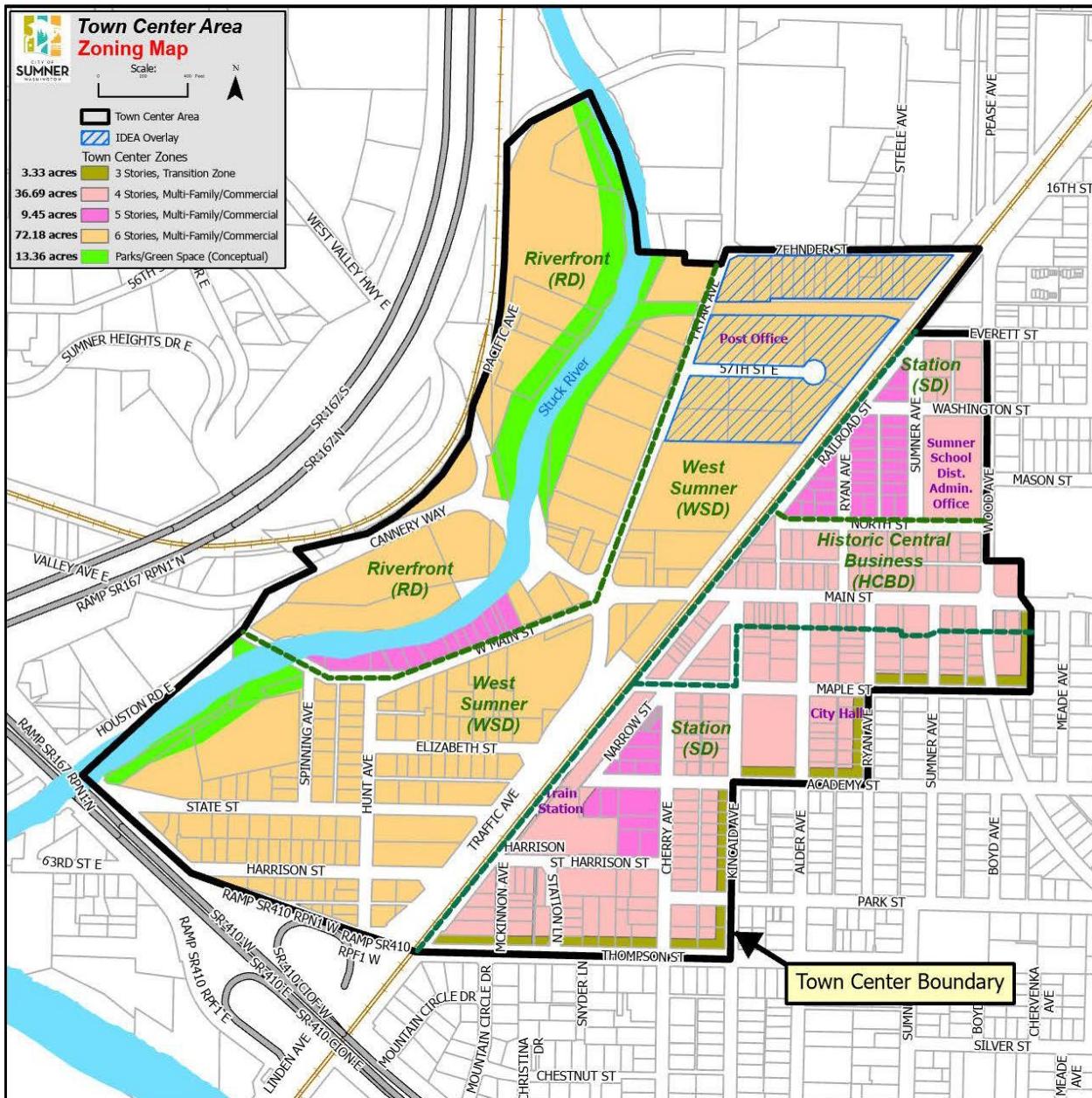
4 Land Use Plans

Land use plans guide private and public development to achieve the City's housing, economic development, and community character goals. This section describes current and proposed Comprehensive Plan and Zoning classes in the Town Center.

4.1 Current Comprehensive Land Use Plan and Zoning Map

The City of Sumner maintains a zoning map consistent with Comprehensive Plan land use designations. The Town Center Plan area is governed by a set of zoning districts and overlays designed to reinforce a vibrant, mixed-use downtown (see Figure 5). The applicable zones, their primary and secondary uses, and geographic extent are summarized in Table 6. In 2018, the City updated the Town Center Plan, consolidating prior zoning classifications into the current Town Center districts. This update continues to build on that foundation, with refinements that respond to monitoring, community input, and emerging needs identified since adoption.

Figure 5. Town Center Plan District Map



Source: Sumner GIS, August 2025

Note: Listed as "Figure 15" in adopted Ordinance 2939, figure titling corrected for easier navigation

Table 6. Zoning Districts and Acres

ZONE	NET ACRES	%	PRIMARY USES	SECONDARY USES
Historic Commercial Business District	10.21	15%	Commercial/mixed-use	Residential on upper floors or attached side/rear
West Sumner District	51.02	36%	Convenience commercial and mixed-uses serving broader market, auto oriented	Residential on upper floors or attached side/rear
Riverfront District	43.70	31%	Commercial/mixed use development	Public and institutional uses
Station District	34.31	24%	Commercial/mixed use development	Multifamily residential at low and moderate densities; such as Accessory dwellings, duplex or 4 plex.
Total	139.24	100%		

Source: Sumner Municipal Code, Title 18

Note: Numbers and percentages are rounded. Acres reported are net and include parcel/property area. If considering roads and river area, the total Town Center Plan Area equals approximately 197 gross acres.

Note: Listed as "Table 5" in adopted Ordinance 2939, table numbering corrected for easier navigation

The majority of the Town Center Planning Area contains existing commercial uses (65%) with the remaining made up of existing medium density residential (18%) and low-density residential (17%) uses. The area in closest walking distance to the Sounder train station were previously zoned Medium Density Residential with a maximum height of 35 feet and maximum density of 15 dwelling units per acre. The current plan designates it within the Station District and would accommodate up to 300 dwelling units.

4.2 Capacity of Proposed Zoning for Added Dwellings

Eight sites are shown in Figure 4 as Phase 1 Opportunity Sites, considered the most likely to redevelop within the next 5–10 years depending on market conditions. Phase 2 Opportunity Sites

are those that may require lot assemblage or face other complexities, making them less likely to redevelop in the near term. Other vacant and development-potential parcels are noted in Figure 4.

Earlier analysis of these opportunity sites suggested that the Town Center Plan area could accommodate approximately 2,308 housing units, a significant increase over the current estimate of 339 units. Under that scenario, employment potential was assumed to remain relatively stable at about 460 employees compared to 408 today.

With the adoption of the 2025 Comprehensive Plan update, Buildable Lands evaluation, and the Town Center Plan refinement, these estimates have been updated. Housing capacity is now projected at approximately 1,768 units, while employment is expected to reach 430 jobs. The difference reflects more detailed parcel-level analysis, updated redevelopment assumptions, and consistency with countywide planning forecasts.

The Comprehensive Plan and Form-Based Code continue to shape growth through form and height rather than explicit density limits. While some adjustments to height standards are under consideration, anticipated densities remain consistent with Pierce County Buildable Lands assumptions—averaging about 76 dwelling units per acre, with effective densities in some locations reaching 110–120 units per acre. Retaining mid-rise building heights of up to six stories ensures that the Town Center remains aligned with regional growth expectations while supporting a walkable, transit-oriented district. See Table 7 and Table 8.

Table 7. Opportunity Sites Acres and Assumed Housing Densities

PROPERTY	ACRES	PROPOSED DENSITIES PER ACRE
Phase 1 Opportunity Sites		
Alder Avenue	0.50	90
Library	5.65	112
Spinning Avenue	5.28	40
57 th Street	2.81	76
Subtotal	14.24	-
Phase II Opportunity Sites		
Zehnder Street	3.73	90
Elizabeth Street/State Street	2.4	90
Pacific Avenue	5.6	112

PROPERTY	ACRES	PROPOSED DENSITIES PER ACRE
Thompson Alder Avenue	1.2	40
Subtotal	12.93	-
Grand Total	27.17	

Note: Listed as "Table 6" in adopted Ordinance 2939, table numbering corrected for easier navigation

Table 8. Housing and Employment Capacity Assumptions

	NET CAPACITY HOUSING UNITS	NET CAPACITY EMPLOYMENT	POPULATION PLANNING PERIOD (2044)
Proposed 2025 Town Center Plan Update: Net Capacity	1,768	430	4,113
Proposed 2025 Town Center Plan Update: Assumed Net Growth 2020-2044	1,245	430	2,897
Proposed 2025 Town Center Plan Update: Assumed Net Growth 2023-2044 accounting for pipeline permits	938	398	2,182
Existing + Assumed Net Growth = 2044 Total	1,549	1,719	3,604

Note: Capacity assumptions are to the planning horizon 2044.

Note: Listed as "Table 7" in adopted Ordinance 2939, table numbering corrected for easier navigation

Housing capacity within the Town Center remains at approximately 1,768 units, though the distribution shifts slightly across opportunity sites. For example, along Zehnder Street, similar housing levels are retained in live/work formats while more employment opportunities are expected. At the Fryar Avenue library site, the White River shoreline and larger redevelopment parcels support mixed-use residential potential. Employment is strengthened in areas such as 57th Street East and within the IDEA Overlay District, which together adds roughly 100 new jobs. Overall, jobs increase from 330 under the 2024 periodic update to 430 under the refined assumptions, while housing remains steady.

4.3 Growth and Development Concept and Placemaking

Measured in activity units (population plus employment), the Town Center outperforms regional benchmarks. In 2023, the Town Center supported about 2,820 activity units (19 per acre). By 2044, activity units are expected to rise to 5,223 (36 per acre) under the current plan, and to 5,323 (37 per acre) with the Town Center plan adjustments. Both levels exceed the Countywide Planning Policy minimums of 10 existing and 26 future activity units per acre.

Puget Sound Regional Council has forecasted the region to grow by about 1.8 million people between now and 2050. Additionally, the region is projected to add about 1.2 million jobs within the same timeframe. To address a small portion of this growth Sumner is looking to the future and where to place the population and job growth. The direction received from the workshops and market studies indicate that growth should be focused near existing transportation infrastructure such as the Sounder commuter rail station.

4.4 Form Based Code

To help attract housing, to both Phase I and other opportunity sites, Comprehensive Land Use Plan and Zoning amendments are recommended. These include a combination of updating the zoning code to a Form Based Code, changing zoning boundaries and development standards and the creation of districts.

The Town Center Form based code(FBC), adopted in 2018, is a key tool for implementing the Town Center Plan and is intended to remove limitation on zoning and allow the market to determine use, while the code focuses on the form and fit of buildings within the Town Center Plan area. The character of the Town Center will be shaped primarily through building form, streetscape design and how these elements relate to the historic Main Street and other parts of the City.

The FBC remains the regulatory framework that provides the scale of development envisioned in the Plan. The code organizes regulations into several layers:

- Districts establish the overall vision and intent for subareas within the Town Center.
- Lot Types specify the range of building types (residential, commercial, or mixed-use) permitted on particular lots within each district.
- Street Types regulate the design of the public realm, including the relationship between buildings and the street.
- Design Requirements and Development Standards provide detail on building height, frontage types, and other elements of building and site design.

The following provides a summary of the proposed Districts, Lot Types, and Public Realm standards.

4.5 Districts

The Town Center Plan area is divided into districts which are unique because of their proximity to Main Street, or the Sounder station, existing low density residential areas and the White River. Because of these factors they have individual goals and development standards as presented in Table 9 and Figure 5.

Table 9. Town Center Area Districts

DISTRICT	LOCATION/EXTENT	DEVELOPMENT STANDARDS
Historic CBD (HCBD)	<p>Bounded on the west by the BNSF railroad, on the east a half a block east of Wood Avenue; on the north by North Street and south to Maple Street near the train station and then about a parcel depth south of Main Street. The HCBD takes in both sides of Main Street. The area totals 17 acres.</p>	<ul style="list-style-type: none"> ▪ Retain high-quality design and historic Main Street character through design guidelines and form. ▪ Form based code governs form and building type and land-use. ▪ Limit height to minimum of 3-4 stories. ▪ Control density by height and open-space and parking standards. ▪ Typical density ranges from 40-112 du/acre. ▪ Maximum 3-story buildings would be required when located in a transition zone as a visual transition from typical 1-2 story single-family to multi-family units. ▪ Existing single-family dwellings would be limited on expansion except for ADUs.
Station District (SD)	<p>District is split with northern area creating a triangle bound by Railroad Street, Everett St, Wood Avenue and North Street; southern area abuts the HCBD to the north, Thompson Street to the south; BNSF railroad tracks to the west and a block east of Wood Avenue. The boundary stair steps along the western boundary following Kincaid Avenue north and south, turning on Academy, east and west, then north again on Ryan Avenue to Maple Street. The SD is 54 acres.</p>	<ul style="list-style-type: none"> ▪ Form based code governs form and building types and land uses. ▪ 4 stories maximum on majority of zone with 5 story buildings allowed closer to the railroad and train station. ▪ Control density by height and open-space and parking standards. ▪ Typical density ranges from 40-112 du/acre. ▪ A transition zone would limit height to 3-story buildings within 30 feet of adjacent low density residential zones to create transition from typical 1-2 story single-family to multi-family units. ▪ Commercial uses would be prohibited within 100 feet of the transition zone. ▪ Existing single-family dwellings would be limited on expansion except for ADUs.

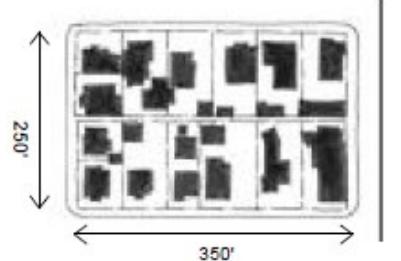
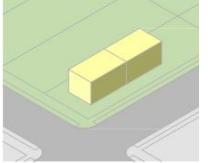
DISTRICT	LOCATION/EXTENT	DEVELOPMENT STANDARDS
West Sumner District (WSD) & IDEA overlay zone	<p>Area is bounded on the east by Traffic/Fryar Avenue, on the west by West Main Street and the White River; and to the east by the BNSF railroad tracks; and extending a whole block depth north of Zehnder Street and to the south SR410. The WSD is 74 acres.</p> <p>The IDEA overlay is added to portions of the WSD, applied to parcels along Zehnder Street and 57th Street</p>	<ul style="list-style-type: none"> ▪ Form based code governs form and building types and land uses. ▪ WSD has 6 stories maximum heights. ▪ Control density by height and open-space and parking standards. ▪ Typical density ranges up to 112. ▪ Development within 200 feet of the White River will also be subject to the Shoreline Master Program. ▪ Existing single-family dwellings would be limited on expansion, except for ADUs. ▪ IDEA Overlay minimum heights are 1-3 stories, maximum 6 stories closer to Fryar Ave. ▪ Parcels south of 57th Street East and within 200 feet of Fryar Avenue would be required to be multifamily or mixed use.
Riverfront District (RD)	<p>This District is bound on the west by West Valley Hwy and Valley Avenue; on the east by West Main Street and Fryar Avenue; on the north by Union Pacific railroad spur and on the south by West Main Street and unimproved right-of-way. The RD is 52 acres.</p>	<ul style="list-style-type: none"> ▪ Control density by height and open-space and parking standards. Typical density ranges up to 112. ▪ Allow height of minimum 4 and maximum 6-story. ▪ Development should be focused on the White River vista and connection. ▪ Allow market to determine use of building (commercial, residential, retail, restaurant) ▪ Existing single-family dwellings would be limited on expansion, except for ADUs.

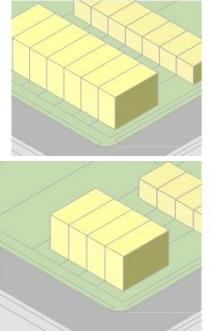
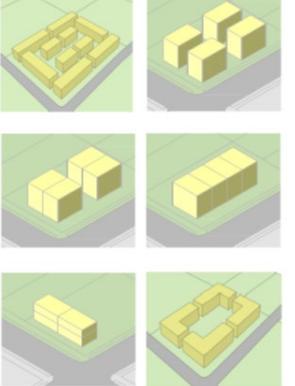
Note: Listed as "Table 8" in adopted Ordinance 2939, table titling corrected for easier navigation

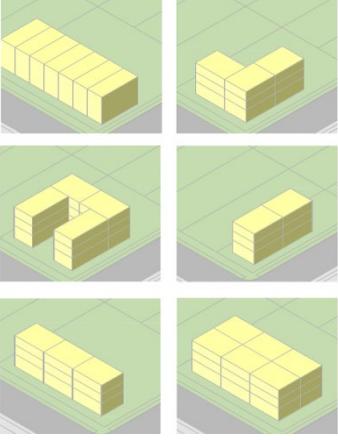
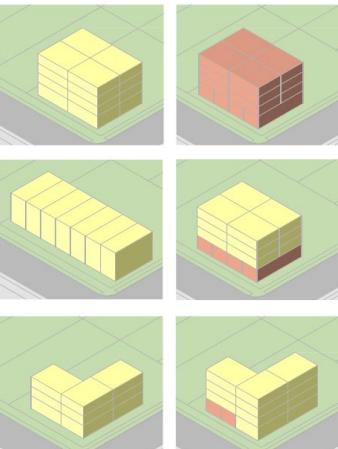
4.6 Lot Types

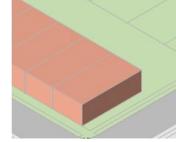
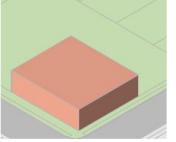
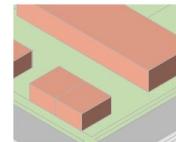
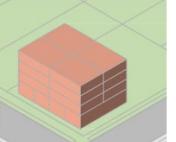
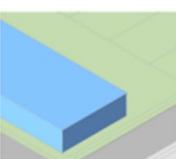
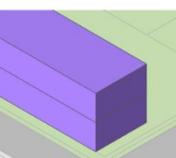
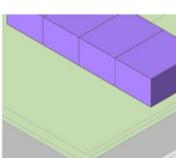
Lot types establish a common vocabulary for the built environment and are used primarily to establish broad intensity and use options for each lot. For example, the Mixed-Use Lot Type allows a mixture of residential and commercial uses on a single lot, while the Multi-Unit Lot Type (5+ units) allows any number of residential units provided they fit within the constraints of the district. Alternative configurations for each lot type are shown to encourage creative building design and to help expand housing options in the Town Center. The configurations shown are not an exhaustive list; additional configurations that fit within the rules of the zone are encouraged. The primary constraint for each lot type is minimum lot size, which applies based on the respective zone. Table 10 provides a summary of Lot Types and descriptions.

Table 10. Lot Types

LOT TYPE/DISTRICTS ALLOWED	DESCRIPTION	DENSITY (DU/ACRE)	CONFIGURATION OPTIONS	CHARACTER EXAMPLES
Existing Conditions Typical Residential Block in Downtown Sumner	Primarily small lot single-family with some accessory dwelling units or small multi-plex. Avg. lot size =6000 SF	6-13 du/acre (typical); up to 21 du/acre possible		
Two-Unit Duplexes Allowed: <ul style="list-style-type: none"> ▪ Station District in Transition Area ▪ Riverfront 	A lot that allows up to two attached dwelling units. Except for home occupations, no commercial activity is allowed.	8-12 du per acre	 	

LOT TYPE/DISTRICTS ALLOWED	DESCRIPTION	DENSITY (DU/ACRE)	CONFIGURATION OPTIONS	CHARACTER EXAMPLES
Residential Attached Townhouse/Rowhouse Allowed: <ul style="list-style-type: none"> ▪ Station District ▪ Riverfront ▪ West Sumner ▪ 	2-3 story attached dwelling units, typically grouped in 4-, 6-, or 8-dwelling groups. Front door faces the street with porch or stoop. Unit sizes = 1600-1800sf. 2 parking stalls-1 garage/1 tandem	16-23 du/acre		
Garden Style Stacked Flats, Walkup Apartments Prohibited in All Districts	Low-rise apartment buildings with 2-5 stories and typically served with surface parking and external stairways	12-15 du/acre		

LOT TYPE/DISTRICTS ALLOWED	DESCRIPTION	DENSITY (DU/ACRE)	CONFIGURATION OPTIONS	CHARACTER EXAMPLES
Multi-Unit (5+ Units) Mid-Rise Urban Apartments Allowed: <ul style="list-style-type: none"> ▪ Station District (only 3 story in Transition Area) ▪ Riverfront ▪ West Sumner 	Apartment buildings constructed “podium style” with 4-5 stories over 1-2 stories of parking.	80-112 du/acre		
Mixed Use Mid-rise Urban Over Commercial Allowed: <ul style="list-style-type: none"> ▪ Station District ▪ Riverfront ▪ West Sumner 	Apartment buildings constructed “podium style” with 4-5 stories over 1-2 stories of parking and commercial uses.	80-112 du/acre		

LOT TYPE/DISTRICTS ALLOWED	DESCRIPTION	DENSITY (DU/ACRE)	CONFIGURATION OPTIONS	CHARACTER EXAMPLES
Commercial Allowed: <ul style="list-style-type: none"> ▪ Station District ▪ Riverfront ▪ West Sumner ▪ 	A lot that allows a variety of permitted commercial uses. No residential uses are allowed.	N/A	   	
Civic Allowed: <ul style="list-style-type: none"> ▪ All Districts 	Civic uses such as city facilities, community spaces, and parks.	N/A	 	
IDEA/Micro-manufacturing Allowed: <ul style="list-style-type: none"> ▪ West Sumner District IDEA Overlay 	A lot type that allows for a mix of multifamily, mixed use, and small scale manufacturing, artisan lofts.	0-76 du/acre	 	

Note: Listed as "Table 9" in adopted Ordinance 2939; table numbering corrected for easier navigation

4.7 Public Realm

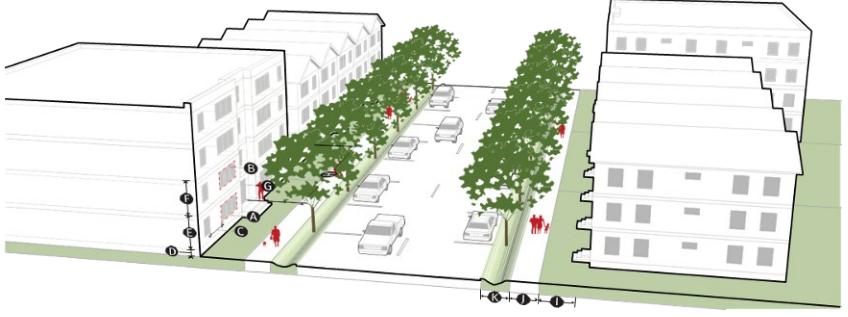
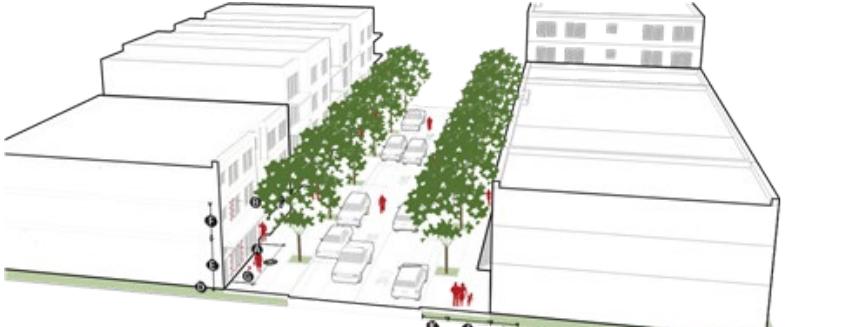
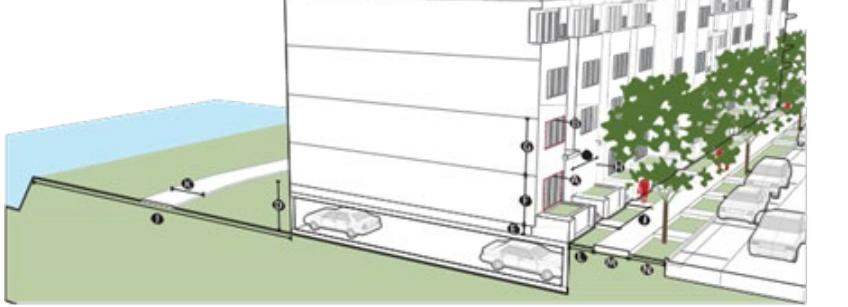
The Public Realm standards determine how the buildings relate to the street in terms of transparency or window area, blank walls, height, setbacks, sidewalk widths and building elements. Each of these is described below. Table 11 describes elements of the public realm that include building design standards, setbacks, and building elements. Table 12 provides illustrations for Public Realm areas.

Table 11. Public Realm Elements

ELEMENT	DESCRIPTION
Transparency	This regulates the amount of window area that should be provided on the façade of the building with the intent to avoid large expanses of blank walls. The FBC works in concert with the Design Guideline.
Story Height	Story height may differ depending on whether the ground floor is commercial, residential or mixed use. Typically ground floor commercial requires a story height minimum of 13 feet. There may be a requirement for increased floor height for residential on the ground floor to ensure a separation between the public sidewalk and private residential spaces in the building.
Setbacks	Setbacks from the primary street and secondary streets are specified. Typically residential attached units are afforded a greater setback than commercial to increase privacy and separation from the street.
Sidewalk Width	The sidewalk width, planting strip, and street trees and spacing is regulated. This pedestrian and street tree furniture zone is a minimum of 12 feet. These details also match the street type.
Building Elements	<p>There are a number of types of building elements and frontage types, some more appropriate for certain street types and districts over others. Potential building elements include:</p> <ul style="list-style-type: none"> ▪ Awning ▪ Balcony ▪ Forecourt ▪ Porch ▪ Stoop

Note: Listed as "Table 10" in adopted Ordinance 2939, table titling corrected for easier navigation

Table 12. Public Realm Examples

LOT TYPE	PUBLIC REALM (TYPICAL)
Mixed Use/ Multi-Unit	
Residential Attached	 <p data-bbox="514 1045 1052 1066">The public realm standards below apply to all streets, unless noted otherwise.</p>
Mixed Use/ Commercial	
Mixed Use/Multi- Unit-Riverfront District	

LOT TYPE	PUBLIC REALM (TYPICAL)
IDEA Overlay -West Sumner District (Zehnder Street Concept)	

Note: Listed as "Table 11" in adopted Ordinance 2939, table numbering corrected for easier navigation

4.8 Activity Nodes and Public Spaces

As the Town Center Plan area develops with greater housing options and larger and taller buildings public spaces that afford places to recreate and gather will become more valuable and essential to maintaining quality of life. The Town Center area is planned to contain public gathering areas at key intersections. The design guidelines will also require private open space areas within developments.

Activity Nodes are identified for future ground floor businesses that bring activity to the street such as retail, restaurants, coffee shops, microbreweries and performing arts. These spaces are envisioned to offer outdoor seating and amenities that create a lively and interesting streetscape.

4.9 Aesthetics: Height and Scale

4.9.1 Views

Sumner is located in the Puyallup River valley, and the Sumner Town Center study area is located adjacent to the confluence of the Puyallup and White Rivers. As a result, the local topography is relatively flat, and significant views are primarily territorial in nature, looking outward to the Cascade foothills in the east and Mount Rainier to the southeast. Mount Rainier is the more significant view, though it is not visible from all locations within the Town Center Plan area due to the presence of existing buildings and trees.

4.9.2 Shading Conditions

Sufficient access to light and air is an important consideration in urban design due to its effect on the experience of area residents and pedestrians. Excessive shading effects can occur where a substantial difference occurs in building heights, where tall buildings are insufficiently set back from

sidewalks and other public spaces, and where architectural designs do not include sufficient variation in building massing.

Due to the relatively low building heights in the study area, existing development in the Town Center generally produces negligible shading effects on residential uses and public spaces.

4.9.3 Transition Areas

Transition areas are identified on the Town Center Plan area map (See Figure 5) indicating areas within 30 feet of low-density residential zoned properties will be limited to 3 stories in height. This limit will soften the transition from 4-5 story buildings and be more characteristic to the adjacent neighborhoods. Transition areas will also be prohibited from containing commercial uses that could be impactful to the low-density residentially-zoned areas.

4.9.4 Height Comparisons

The Supplemental Environmental Impact Statement being prepared for the Town Center Plan provides illustrations and analysis of the increased heights in the various areas of the Town Center below is an example of Main Street and imagery that contains a "glass boxes" depicting the proposed increased heights as a shadow against streetscapes with existing buildings. The Supplemental EIS being prepared for the Town Center Plan Update contains more detailed illustration for other streets in the Plan area.

Figure 6. Street View – Main Street at Kincaid Avenue Looking East (4 stories)



Imagery Source: Google Earth

Note: Listed as "Image 1" in adopted Ordinance 2939, and modified to Figure numbering for easier navigation

4.9.5 Character and Design

The Town Center Plan area contains a mix of existing building types, streetscapes, historic character and small-town look and feel that is valuable asset. The following illustrations are conceptual drawing at key intersections in the Town Center Plan area and are intended to provide a sense of scale of new development. While historic character and structures are important elements in the downtown, new development will not be required to be a historic replica, but rather to incorporate elements of good design such as pedestrian oriented building fronts, use of natural wood and brick materials, and traditional window placements

Figure 7. Historic CBD- Design and Character example - 909 Main Street



Note: Listed as “Image 2” in adopted Ordinance 2939, and modified to Figure numbering for easier navigation

Figure 8. Example Mixed Commercial and Residential Possibilities on Red Apple Block along Maple Street and Alder Avenue



Source: Google Earth, City of Sumner 2018

Note: Listed as "Image 3" in adopted Ordinance 2939, and modified to Figure numbering for easier navigation

Figure 9. Example Residential on Thompson Street and Kincaid Avenue



Source: City of Sumner 2017

Note: Listed as "Figure 1" in adopted Ordinance 2939, figure numbering corrected for easier navigation

Figure 10. Example Potential Commercial and Residential Development on State Street and Traffic Avenue



Source: City of Sumner, 2017

Note: Listed as "Figure 2" in adopted Ordinance 2939, figure titling corrected for easier navigation

Figure 11. Example of Woonerf on West Main Street with Mix of Commercial and Residential



Source: City of Sumner, 2018

Note: Listed as "Image 4" in adopted Ordinance 2939, figure titling corrected for easier navigation

4.10 Nonconforming Land Uses

The vision for the Town Center Plan area includes increased housing opportunities, mixed use commercial/residential where appropriate, and will be marked by increased heights from 2-3 stories within the Downtown. The first several years of development will be on Opportunity Sites which are larger or underused properties identified in the Land Use chapter as prime locations for new development. These sites are expected to be built out to the maximum building heights allowed in the zoning code. Encouraging density on these sites will help meet housing needs, support mixed-use development, and create momentum for the Town Center. As these key sites are developed, they will set the stage for future redevelopment of nearby properties over time. In order for the Plan to be implemented as envisioned it will require the acquisition and assembly of multiple parcels that are currently small lot single-family residential. The redevelopment of these areas, including parcel acquisition and assembly, may become more complex if properties are expanded or redeveloped in a manner that increases their long-term use as standalone single-family homes. However, consistent with state legislation (HB 1337, 2025), allowing accessory dwelling units (ADUs) on existing single-family lots provides an opportunity to add housing diversity and support incremental increases in property value, while maintaining the overall vision for future midrise urban housing and mixed-use development. To balance these objectives, limitations consistent with current regulations are proposed for legally nonconforming single-family residential properties as follows:

- Allow single-family residential to be permitted use to avoid impact on financing and insurance.
- Allow expansions up to 25 percent of the floor area, except that expansion shall not occur if it is necessary to purchase additional property. The expansion shall meet the development standards of the zone such as setbacks, lot coverage, and building height.
- Consistent with HB 1337, allow existing single-family dwellings to add up to two accessory dwelling units (one attached and one detached), provided they meet applicable state and local ADU development standards.
- No additional primary dwelling units may be added, except that the existing single-family dwellings may add an accessory dwelling unit, (up to 2 ADU's per state law).
- Structures may be rebuilt after a fire or other disaster to original dimensions unless a health or safety impact would occur, provided a complete building permit application has been submitted within three years.

4.11 IDEA Overlay Transition Zone

The 2025 Town Center Plan introduces the **Innovation District and Enterprise Area (IDEA) Overlay zone**, located on Zehnder Street and 57th Street within the West Sumner District of the Town Center. The IDEA District builds upon the FBC framework while tailoring standards to support small-scale manufacturing, artisan enterprises, creative industries, and entrepreneurial uses not previously emphasized in the Town Center Plan.

New opportunity sites in the IDEA Overlay District will be required to incorporate design improvements to the street and sidewalks, including accommodations for on-street parking, bicycle lanes, and widened sidewalks along the southern frontage. Updated FBC standards ensure these improvements are coordinated with adjacent development and reinforce Sumner's goals for placemaking, walkability, and economic vitality.

By combining flexible land uses with high-quality urban design standards, the IDEA District serves as both an incubator for innovation and a bridge between Sumner's historic small-town character and its future as a thriving mixed-use, transit-oriented center.

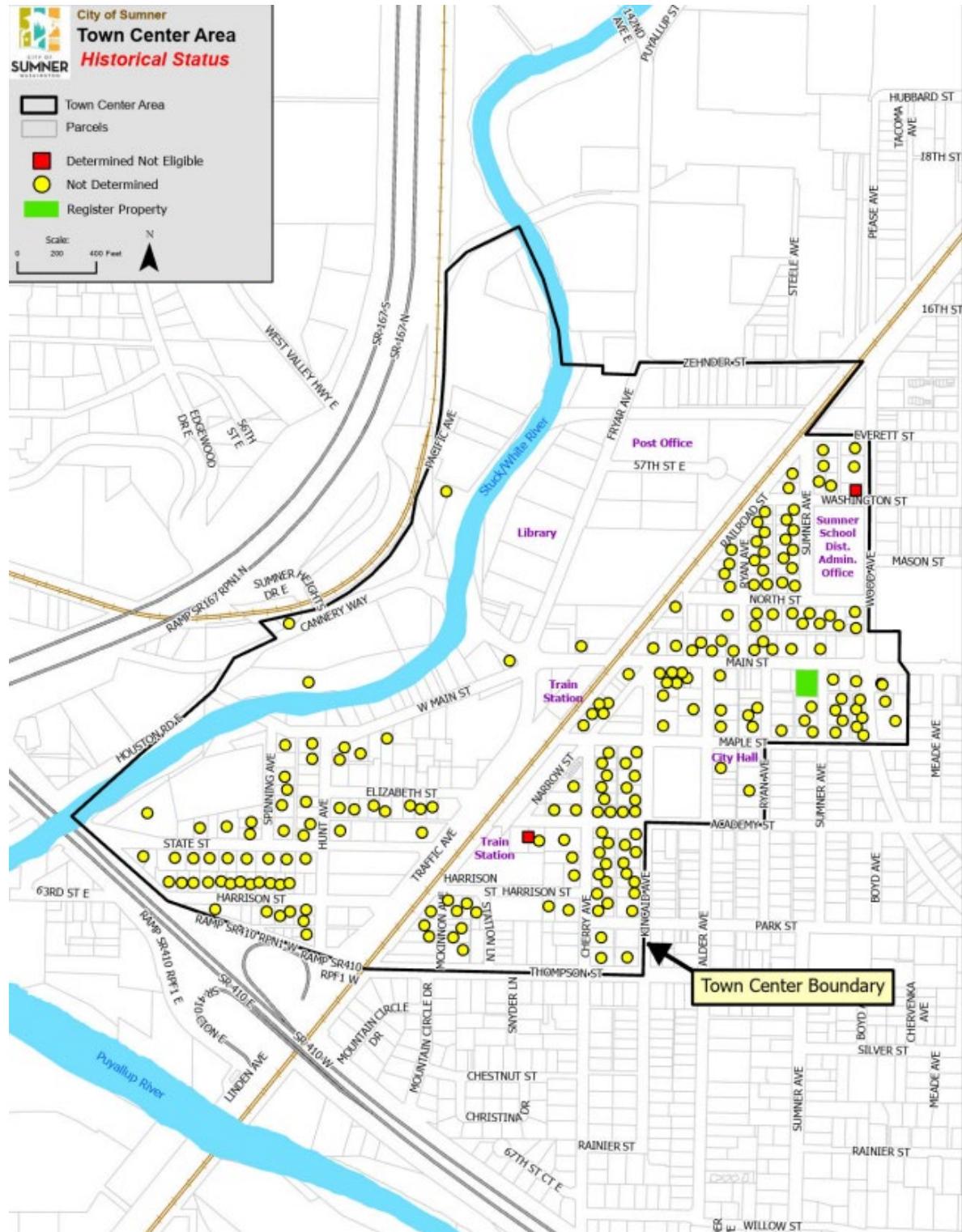
5 Cultural Resources

The Puyallup and White River valley was home to the Puyallup and Muckleshoot tribes since time immemorial and the lands encompassed by the Town Center were customary and traditional fishing and hunting areas. Following settlement by the pioneers in 1853. In 1875 the first post office was established in Sumner and the town was platted by George Ryan in 1883. Sumner incorporated in 1891 and began as a farming community centered on the town site. Additional development evolved as the community grew to service the surrounding agricultural area.

The Ryans lived in a house right on Main Street, located on the southwest corner of Main and Sumner Avenue, and was the only designated historic landmark in the Plan Area. It was placed on the National Register of Historic Places in 1977. George Ryan and his wife were important leaders in the growth of the town. Mrs. Ryan was the first post-mistress, and the post office was located at the Ryan home. Mr. Ryan was the City's first mayor. He also opened the community's first general store, constructed the first two-story brick building on Main Street, helped extend Main Street, and built the City's first railroad depot. The House was donated to the City for use as a library in the 1920s. When a modern library was built on Fryar Avenue in 1979, the facility became a museum for decades and was operated by the Sumner Historical Society. Unfavorably, the Ryan House was demolished in 2025 following years of effort and lack of adequate funding secured to restore the structure. The building was structurally unsound and not fit to be restored into a public space. Furthermore, the Ryan family deeded the house to the city with stipulations that when the House was removed the land be developed as a park in honor of George Ryan's wife and respectfully named Lucy V. Ryan Park.

The map in Figure 12 shows various undetermined historical sites in the Town Center Plan area. Two sites in the Town Center that were determined eligible for the National Historic Register prior to the 2018 update, including the Ryan House and Bridge Street Bridge that was replaced by a newer bridge and dismantled in 2019. Lucy V. Ryan Park remains a registered site.

Figure 12. Historical Status



Source: Sumner GIS, August 2025

Note: Listed as "Figure 20" in adopted Ordinance 2939, figure titling corrected for easier navigation

Numerous buildings built in the 1800s and early 1900s are within the Town Center Plan area but have not been reviewed for eligibility and are not registered.

Three sites in the Town Center were reviewed and determined to be not eligible:

- Riverside Pub and Eatery—13704 Valley Avenue E (Since demolished)
- Sumner Auto Collision and Repair—725 West Main Street
- Winsome Grace—1313 Washington Street

In 1982/1983, Pierce County conducted a cultural resources survey of approximately 61 buildings citywide. The blocks within the Plan Area studied include:

- Main Street and West Main Street
- Wood Avenue
- Cherry Avenue
- Ryan Avenue
- Thompson Street
- Washington Street
- Elizabeth Street
- State Street

No buildings surveyed in the Plan Area were nominated for the National or State historic registries in response to the County survey. However, the structures provide a historic character reference for the Town Center, especially in the commercial blocks on Main Street between Railroad Street and Wood Avenue. While many of these commercial buildings on Main Street have been altered, collectively they convey a historic context for the community. The commercial buildings surveyed in 1982/1983 on Main Street include:

▪ 909 Main Street	▪ 1101 Main Street
▪ 1004 Main Street	▪ 1109 Main Street
▪ 1005 Main Street	▪ 1202 Main Street
▪ 1006 Main Street	▪ 1207 Main Street
▪ 1016 Main Street	▪ Northwest Corner of Main Street and Ryan Avenue
▪ 1022 Main Street	

Other commercial and residential buildings that were surveyed in 1982/1983 in the Plan Area include:

- NE Corner of Thompson Street and McKinnon Avenue
- 800 Traffic Avenue
- 732 Ryan Avenue (SW corner of Ryan Avenue and Academy Street)*
- 1204 Washington Street
- 809 Cherry Avenue
- 817 Cherry Avenue
- 816 Cherry Avenue*
- 802 Cherry Avenue
- 704 Cherry Avenue
- 918 Cherry Avenue*
- 634 Elizabeth Street
- 621 Elizabeth Street
- 617 West Main Street
- 638 Elizabeth Street
- 905 Sumner Avenue
- 1104 Maple Street
- 1020 Ryan Avenue
- Southeast corner of State Street and Spinning Avenue

*Address changes may have occurred for these properties.

Many of the buildings in the 1982/1983 cultural resources survey had been altered to varying degrees. Since the survey is 35+ years old, an updated survey of the above-listed properties, including those on Main Street, would be needed to determine if the originally surveyed buildings still maintain their historic features.

Of the buildings listed above, some have been demolished or have undergone extensive changes. These include:

- 816 Cherry Avenue (appears to have been removed, unknown time period)
- 1202 Wood Avenue (demolished in 2004)
- 1022 Main Street (burned and rebuilt)
- 1104 Maple Street (major addition in 2001)
- 1003 Main Street (burned down in 2022)

In 2008 the City adopted a Historic Preservation Ordinance that created a process for properties to be designated and placed on the Sumner Historic Register. The Historic Preservation Ordinance also sets forth a Historic Preservation Commission who reviews applications for registration and issue certificates of appropriateness when changes are proposed to the structures. No structures have yet been designated under the local ordinance.

6 Natural Areas

Environmentally critical areas receive special attention in City policies and regulations in conformance with State and Federal laws. Since the Town Center Planning Area is largely developed, the extent of critical areas is relatively less than in developing parts of the community. Critical areas include the White River and associated 100-year flood zones shown on Figure 13.

Figure 13. Town Center Critical Areas Map



Source: City of Sumner GIS, 2003

Note: Listed as "Figure 17" in adopted Ordinance 2939, figure titling corrected for easier navigation

7 Parks and Recreation

7.1 Parks and Recreation

The Town Center Plan area boasts two rivers and a regional trail connection and two public park areas. The future plans for the area are specified in the recently updated Parks and Trail Plan.

7.2 Reuben Knoblauch Heritage Park

The Reuben Knoblauch Heritage Park was designed and constructed in 1995-96 on a triangular block bordered by Main Street, Kincaid Avenue and Cherry Avenue. Built primarily with materials and labor donated by local individuals, families, and businesses, the park was initially made possible by a generous contribution by the late State Senator Knoblauch, who served in the Washington State Legislature for 30 years before retiring in 1977. The Senator's contribution made possible the purchase of the land where the park was built, along with additional items for the park.

This half acre downtown park has become a focal point for many community and family activities, including concerts, movie nights, family gatherings, stage performances, community festivals, and weddings.

In 2023, following adoption of the Main Street Visioning Plan in 2021, the streets bordering the park were reconstructed to create a “woonerf” street that has no curbs, decorative pavement design and bollards for blocking the streets during events. There were also significant utility upgrades associated with this project. In 2025 the Hops Alley and norther portion of the park were constructed.

The property south of the park, the former post office and accompanying parking lot, were purchased by the City in 2022. The City is currently finishing final designs for a new community gathering place building that will house an event space, activated during the day by a coffee shop or similar business, City Council Chambers, and a few city offices.

7.3 Rhubarb Alley

Rhubarb Alley is a second smaller public gathering space that underwent extensive upgrades in 2011 through a partnership with Tully's Coffee (Green Mountain Coffee Roasters) and the Pomegranate Center. In the summer of 2011, following a short but intense public design process, the alley was furnished with seating, planters containing hops, whimsical sculptures and a small fold-down stage. The wood material was donated by Manke Lumber and designed and constructed by the community with the help from expert craftspeople volunteering through the Pomegranate Center. In 2015, the City finished the alley with stamped asphalt in a redbrick design. The Rhubarb Alley is used for outdoor seating and as a small venue for street events.

7.4 Trackside Alley

Similarly, the alley adjacent to a renovated building at 1114 Main Street was closed and the building owner, in partnership with the City, installed pavers and provided tables for seating. The City installed street lights and electrical power. This space is also available for seating for the public and is a favorite for patrons to Trackside Pizza and serves as a small venue during street events.

7.5 Lucy V. Ryan Park

The site of the former Ryan House was deeded and donated to the City in 1926 from the Ryan Family. The structure served as a local library for 50 years and as a historic museum for another 40 years. In 2023, a city planned renovation determined the house to be structurally inadequate with major work to restore the building outweighed the funding available. The City embarked on a challenging conversation to demolish the unsafe structure and submitted to the Department of Archaeology and Historic Preservation environmental review for the demolition of the Ryan House, which occurred in 2025. The city is following the original deed to redevelop as a full park. A form of artwork/signage will commemorate Lucy V. Ryan and the former house.

The Town Center Project and Action list promotes the identification and creation of additional Downtown green space areas.

7.6 Riverside Park

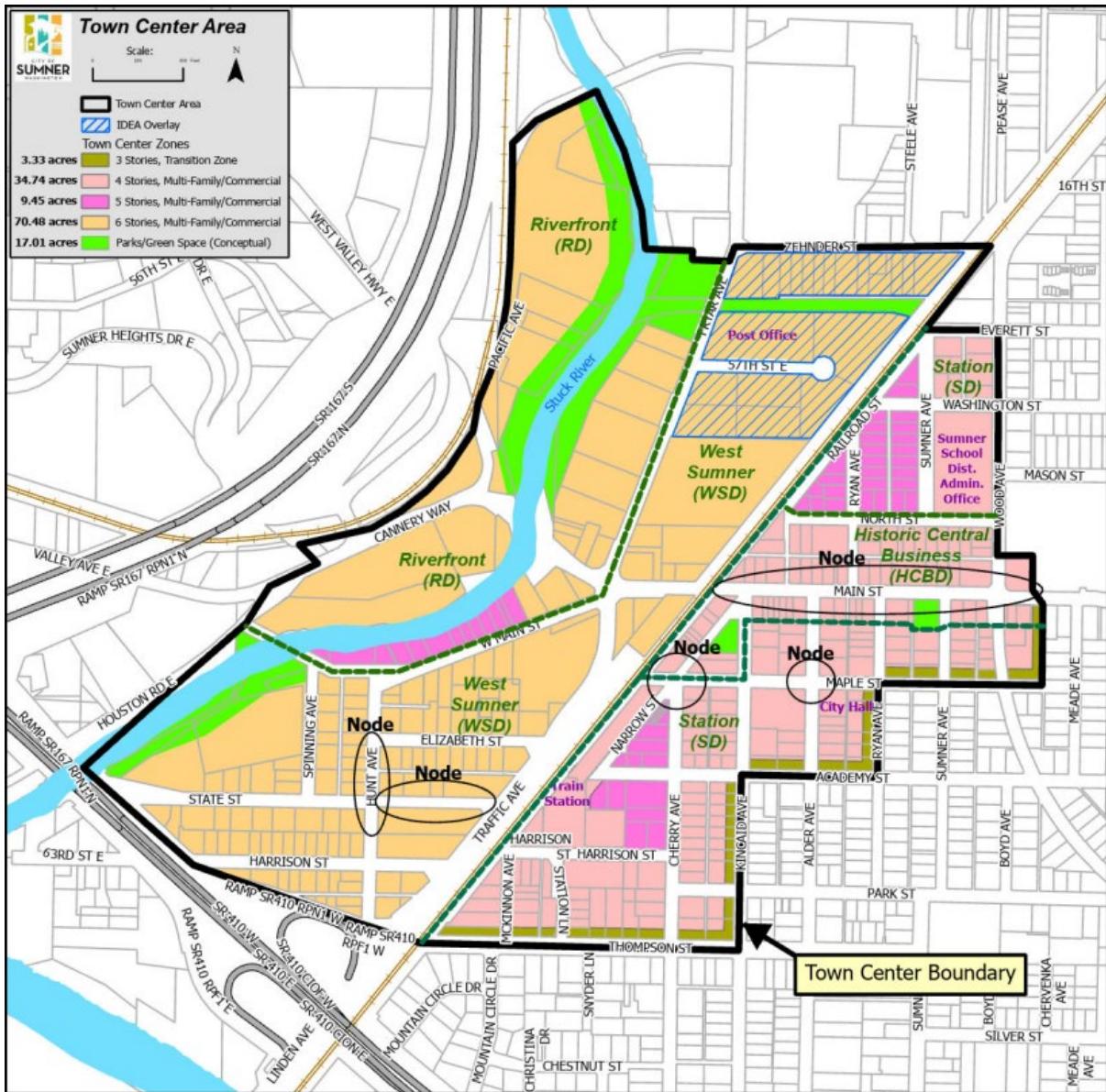
The Town Center Plan anticipates future park space being developed along the White River in West Sumner. This space would contain the current Sumner Link Trail and would provide a visual and accessible connection to the White River. In addition, developments in the Riverfront District would also have views and access to this amenity.

7.7 Future Parks and Connections

The Town Center Plan map in Figure 14 shows potential future open space, parks and trail connections, including potential use of Puget Sound Energy land for trail access to the northern section of the plan area crossing Fryar Avenue. Additional undeveloped areas adjacent to this corridor could be considered for park and open space.

City of Sumner Town Center Plan

Figure 14. Parks and Open Space plan



Source: Sumner GIS, August 2025

Note: Listed as "Figure 16" in adopted Ordinance 2939, figure titling corrected for easier navigation

7.8 Trails

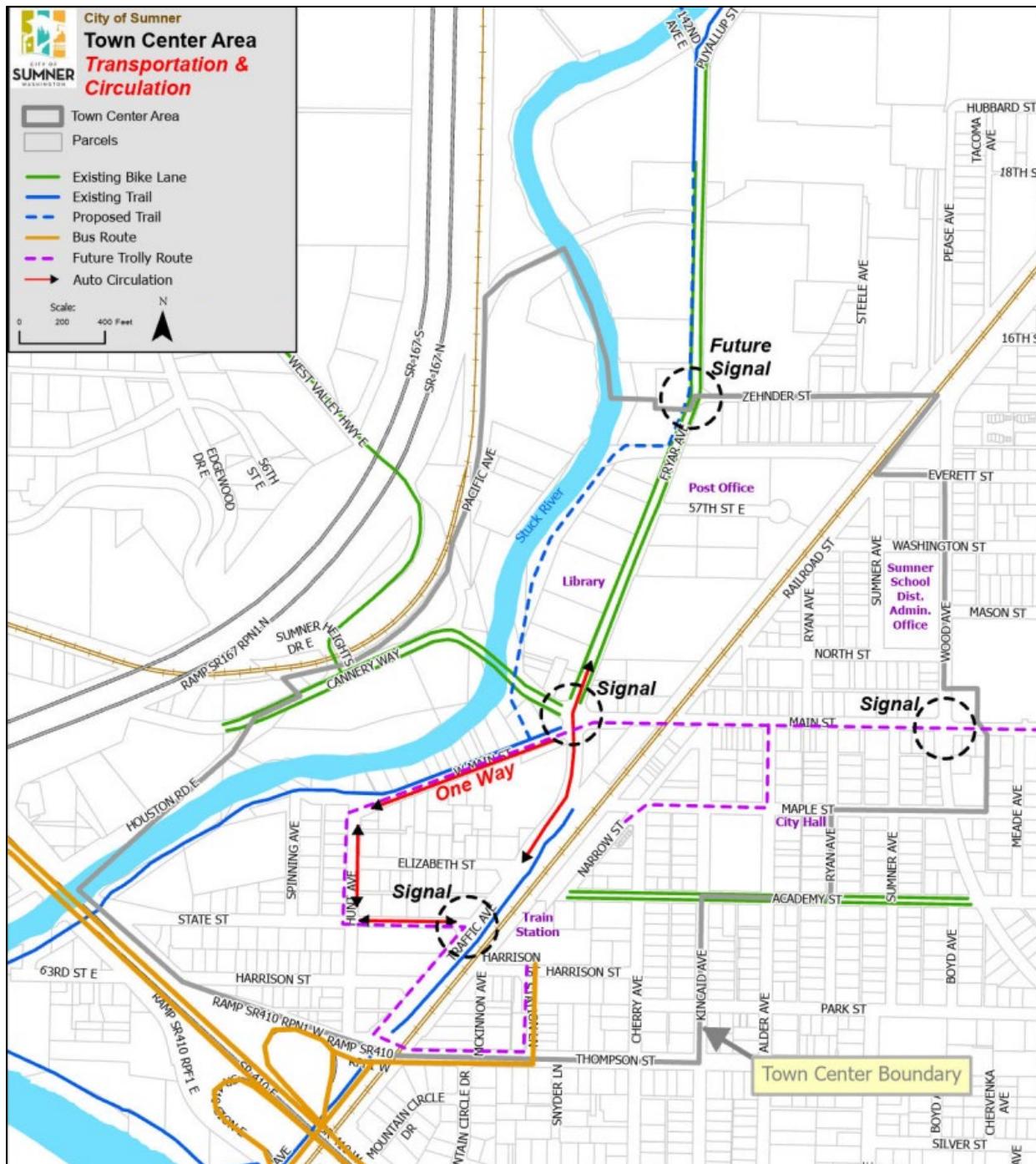
The Transportation and Circulation Map in Figure 15 shows the existing Sumner Link Trail and connections to the Puyallup River trail and future connections to the Foothills Trail that runs south to Orting and South Prairie. To the north the Sumner Link Trail continues for 5 miles winding in proximity to the White River and connecting to Stewart Road and ultimately to trail and bike routes in the City of Pacific. The trailhead for the Interurban Trail is in the City of Algona and from there the trail connects to Seattle. Sumner residents can travel on the trail in either direction for miles exploring the region.

The 2024 Transportation Plan adopts the 20-year Transportation Improvement Plan and includes the following trail and multi-modal improvements that would increase access to and through the Town Center:

- Fryar Ave. Trail. Project is to complete trail connection through town, from West Main Street to Puyallup Street. Design is completing in 2025, and construction is planned for 2026
- Elm St. & Wood Ave Sidewalks. Project plans to extend a multi-modal sidewalk on the north side of Elm Street and the West side of Wood Avenue, from Bonney Ave to Zehnder Street. Project design is planned for 2028, with an estimate construction by 2030.

Plans for non-motorized access include designated bike routes and bike-friendly local streets that allow residents to safely navigate the downtown area. As part of the Town Center Plan, a new east-west bike facility is recommended along Zehnder Street, connecting a future trail on Wood Avenue to Fryar Avenue that would significantly improve cross-town bicycle and pedestrian movement. Additionally, new bike lanes have been constructed along Academy Street and Wood Avenue within the Town Center Plan area, further enhancing the multi-modal network and supporting safe, continuous bike travel throughout the district.

Figure 15. Transportation and Circulation Map



Source: Sumner GIS, August 2025

Note: Listed as "Figure 18" in adopted Ordinance 2939, figure titling corrected for easier navigation

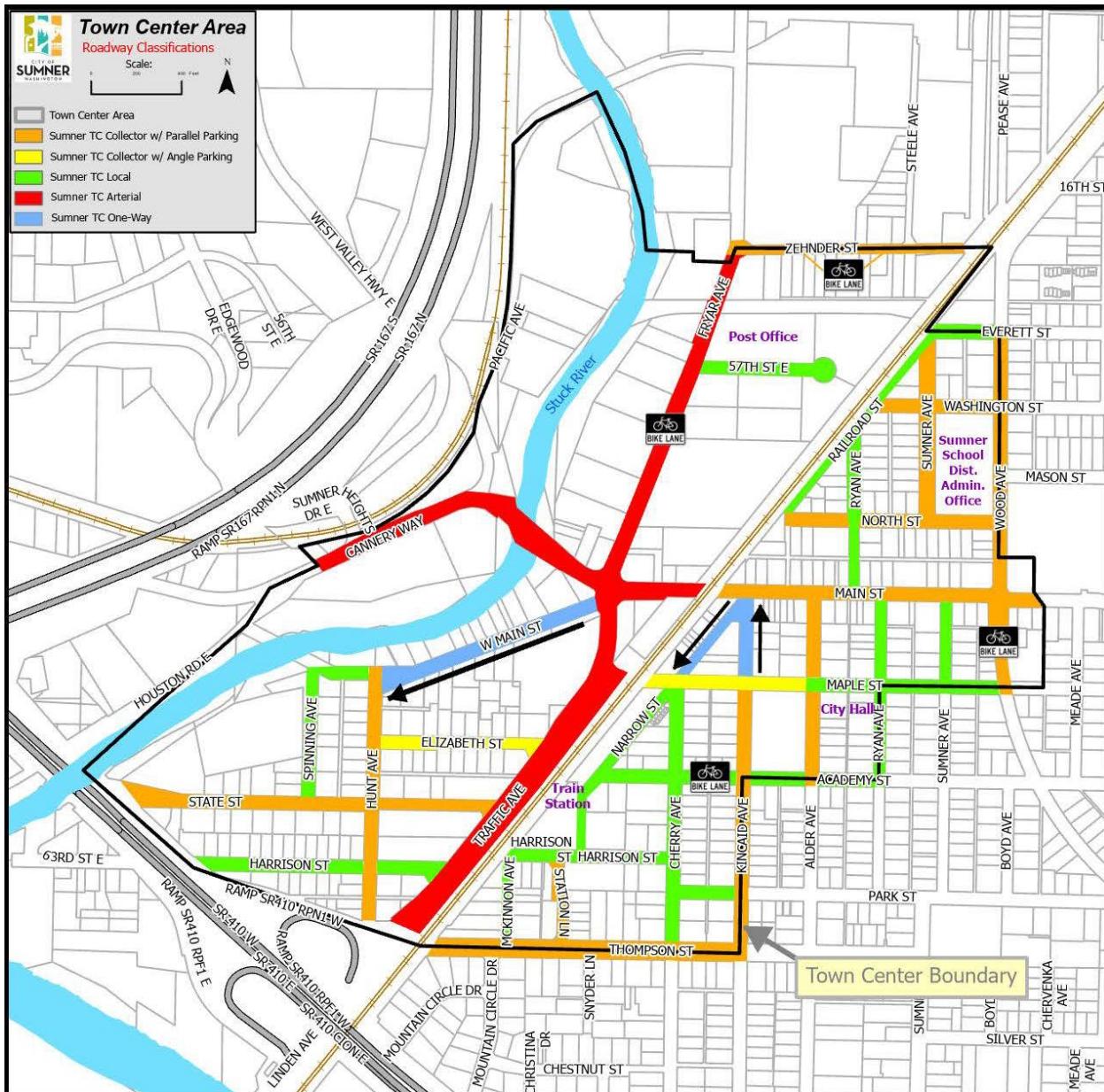
8 Transportation/Circulation

8.1 Street Design

Street designs are essential to creating the neighborhoods and thoroughfares that instill quality experience and enhance the community. Streets that accommodate many types of transportation options are pedestrian and transit friendly and have amenities such as public spaces and street trees to create an environment that is welcoming and livable. The form-based code approach includes specific street designs for certain streets within the Town Center Plan area which are mapped in Figure 16 (Street Type Map). Further illustrations and descriptions are presented below.

City of Sumner Town Center Plan

Figure 16. Town Center Street Type Map



Source: Sumner GIS, August 2025

Note: Listed as "Figure 19" in adopted Ordinance 2939, figure titling corrected for easier navigation

8.2 Arterial Streets

Arterial streets provide primary mobility and connections between Sumner and surrounding cities. Within the Town Center Plan area, Traffic Avenue and Fryar Avenue function as principal arterials running north-south and connecting the industrial area to SR 410. Traffic Avenue is designed as a five-lane arterial requiring a minimum 90-foot right-of-way (ROW), while Fryar Avenue, north of Main Street, functions as a three-lane arterial with a minimum 70-foot ROW. Main Street, west of the railroad tracks, is also classified as a three-lane arterial requiring at minimum 70-feet of ROW and continues westward where it becomes Cannery Way. Per WSDOT standards, these arterials carry significant truck and commuter traffic through the area in addition to serving residents and businesses. Given the high volumes and heavy vehicle use, these corridors are challenging for pedestrians and bicyclists. Figure 17 illustrates a cross-section with widened sidewalks to and planting strips to buffer pedestrians from vehicles. The public realm range allows for portions of trail segment to be developed along Fryar. The following arterial streets are labeled with a ROW range to accommodate trail widths, center lane locations and future public realm improvements.

Right-of-way width:

70-100 ft.

Main Street (west of tracks)	70-80 ft. ROW
Cannery Way	70-80 ft. ROW
Fryar Ave	70-80 ft. ROW
Traffic Ave	80-100 ft ROW

Figure 17. Arterial Street



Note: Listed as "Figure 4" in adopted Ordinance 2939, figure titling corrected for easier navigation

8.3 Collector Streets

Collector streets are intended to consolidate local street traffic and provide primary connections to the arterial network. They typically carry greater volumes of traffic than local streets and serve as key multimodal corridors within the Town Center. Collector streets are designed to support multiple modes of travel. In addition to accommodating vehicular traffic, they include bike lanes and pedestrian facilities consistent with the Plan's multimodal goals. Examples of collector streets in the Town Center Plan area include Main Street, Maple Street east of the railroad, State Street (which has a single light at Traffic Avenue), Elizabeth Street west of Traffic Avenue, as well as Thompson Street (which leads to SR410) and Zehnder Street. Zehnder Street, which forms the northern boundary of the Town Center Plan area, has an existing 40-foot right-of-way. To accommodate parallel parking on the south side, future development will dedicate an additional 10 feet of ROW. Instead of separated bike lanes, a 10-foot minimum multi-modal pathway is envisioned along the north side. Future ROW acquisitions and an update to the Transportation Plan will evaluate the additional ROW needed to extend this pathway west to Wood Avenue. Along with the pathway, future improvements to Zehnder Street are expected to include on-street parking and a minimum 6-foot sidewalk with a reduction on the landscaped/street tree furniture width expected. As illustrated in Figure 18 and Figure 19, two on-street parking configurations are provided for collectors: angled parking and parallel parking. Figure 20 illustrates Zehnder Street and dimensions for parallel parking on one side. Where angled parking is used, it should be configured as back-in angled parking, which is safer than traditional front-in designs and nearly doubles the on-street parking supply compared to parallel parking.

Right-of-way width:

70-80 ft. (angle parking)

60 ft. (parallel parking)

Figure 18. Collector Street with Back-in Angled Parking



Note: Listed as "Figure 5" in adopted Ordinance 2939, figure titling corrected for easier navigation

Figure 19. Collector Street with Parallel Parking



Note: Listed as "Figure 6" in adopted Ordinance 2939, figure titling corrected for easier navigation

Figure 20. Collector Zehnder Street



Note: Listed as "Figure 7" in adopted Ordinance 2939, figure titling corrected for easier navigation

8.4 Local Streets

Local streets have the lowest traffic volumes and are typically located in residential areas. In the Town Center Plan area, they also provide a transition between multi-family developments up to three stories in height and lower-density development of one to two stories. The standard local street cross-section (see Figure 21) is based on a 60-foot right-of-way and includes sidewalks, planter strips, street trees, and parallel parking. Academy Street is an example of this standard local street type, with a planned bike path incorporated within the 60-foot ROW with parallel parking on one side every other street. In comparison, narrower local streets may be as small as 50 feet; for example, on Ryan Avenue, parallel parking is limited to one side (Figure 22). Regardless of narrower conditions, the 60-foot ROW remains the minimum standard for local streets in the Plan.

Right-of-way width:

50-60 ft.

Figure 21. Local Street Standard



Note: Listed as "Figure 8" in adopted Ordinance 2939, figure titling corrected for easier navigation

Figure 22. Narrower Local Street



Note: Listed as "Figure 9" in adopted Ordinance 2939, figure titling corrected for easier navigation

8.5 West Main Street One-way

West Main Street lies between Traffic Avenue and Hunt Avenue and is separated from the White River by a single line of residential housing on north side of the road. This unique section of street was historically the primary thoroughfare between Sumner's downtown and the City of Puyallup before Traffic Avenue and the SR410 freeways was constructed. The north side of the street contains single-family houses with views of the White River and a mix of multi-family and commercial to the south. As shown in Figure 23, this street would be one-way from east to west and retain the right-in from Traffic Ave and continue toward Hunt Ave, with vehicle exit options on Elizabeth Street and State Street. The proposed street design takes advantage of the location of the Sumner Links Trail to accommodate bicyclists, tourists, and pedestrians and contains angled parking to create a walkable and accessible area.

Key Amenities:

Right of way width: 60 ft.

- Sumner Links trail
- One way

Figure 23. West Main Street One-way (Concept)



Note: Listed as “Figure 10” in adopted Ordinance 2939, figure titling corrected for easier navigation

8.6 Streets

The Town Center contains key north-south and east-west arterial streets serving the Sumner community as a whole and connecting to regional transportation routes, SR-410 and SR-167:

- Traffic Avenue/Fryar Avenue
- Main Street

A number of collector arterials provide connections between the residential areas in Sumner, arterials, and the regional freeway system:

- Zehnder Street
- Alder Avenue
- Thompson Street
- Wood Avenue
- Bridge Street/Valley Avenue East

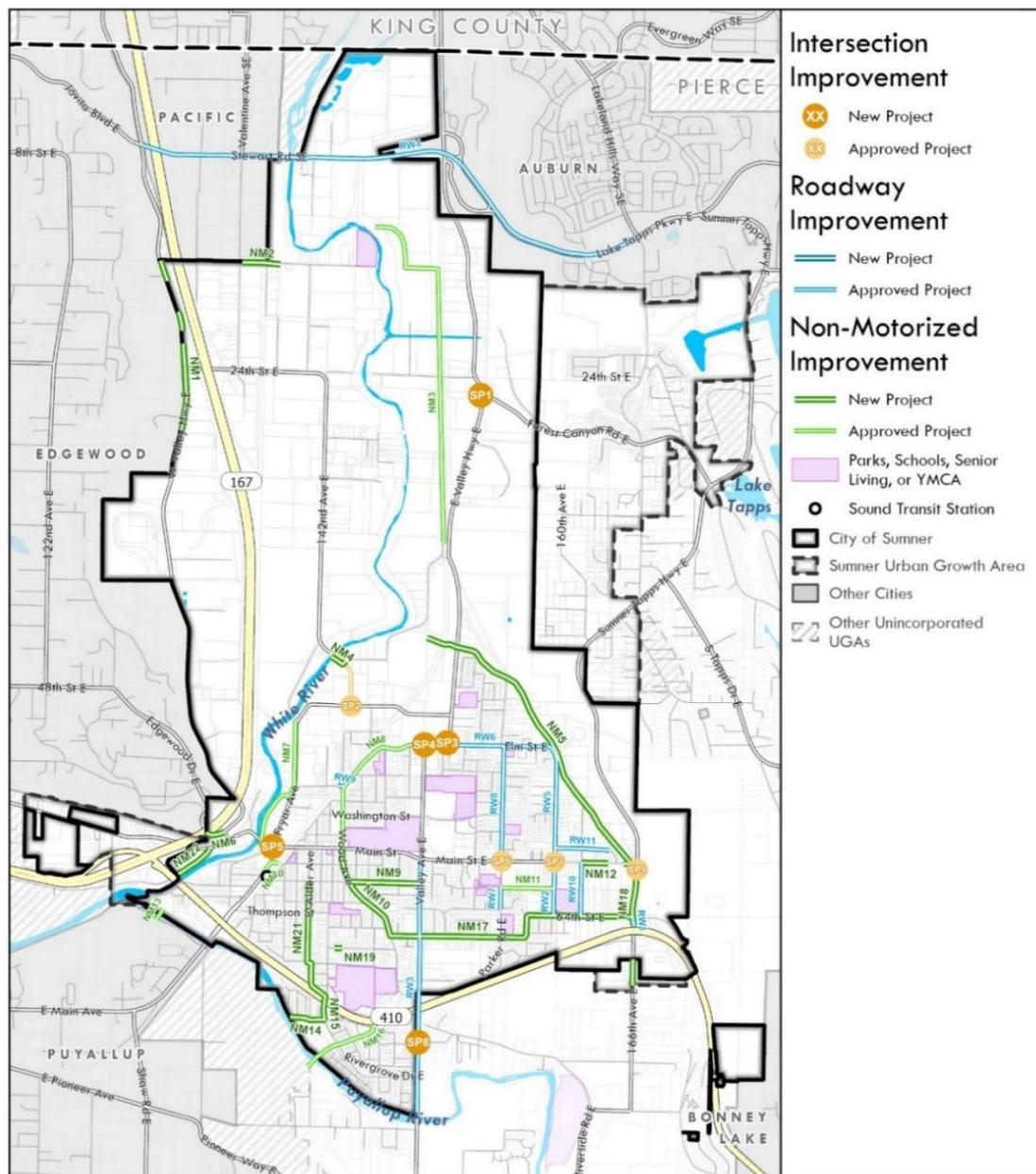
The remaining roadways in the Town Center Planning Area are classified as “local streets” and primarily provide for property access into Sumner. While currently many local streets generally have two travel lanes, and provide access between residential or business areas and the arterials, they do

not provide the desired results for pedestrian-oriented corridors. Through the Town Center Plan all streets will be reclassified and focus will be provided on pedestrians and multi-modal transportation options and less on the automobile.

8.7 Planned Improvements

The planned transportation improvements within and around the Town Center reflect a coordinated effort between the City, Pierce County, and the State to address future mobility needs. Projects identified in the Transportation Plan and regional programs are anticipated to be funded and constructed by 2044, and have been incorporated into the future roadway network analysis. These improvements are intended to enhance multimodal access, reduce congestion at key intersections, and support the Town Center's role as a walkable, transit-oriented hub.

Figure 24. Transportation Management Plan: 20-year Improvement Projects 2024-2044



**CITY OF SUMNER
20-Year Improvement Projects**



Disclaimer: Map features are approximate only. The City of Sumner does not guarantee the accuracy of this map nor assume any liability from the use of or reliance on the information herein.

Map Date: February 2024

Source: City of Sumner, 2023; Transpo, 2024.

Note: Listed as Transportation Management Plan Exhibit 8-1 in adopted Ordinance 2939, figure titling corrected for easier navigation

Key transportation improvements in the vicinity of the Town Center include:

- SP5 Traffic Avenue/Main Street: Add EB right-turn overlap. Convert W Main Street to one-way facility westbound.
- RW9 Zehnder Street: Pease Avenue to Wood Avenue Railroad Crossing Improvements
- NM6 Edgewood Drive Sidewalks: Complete missing sidewalk facilities between SR 167 and Sumner Heights Drive E
- NM7 Fryar Avenue Trail (WSDOT SUM-17): West Main Street to Puyallup Street
- NM8 Zehnder/Elm Street: Construct sidewalks and bike lanes (Valley Ave–Main St; Pease–Wright).
- NM20 Traffic Avenue Pedestrian Signal (WSDOT SUM-25): Replace existing pedestrian rectangular rapid flashing
- NM21 Alder Avenue Sidewalks: Construct pedestrian and bike facilities between SR 410 and Academy Street.
- NM22 Houston Road E Sidewalks: Construct pedestrian facilities between Valley Avenue E and the west City limits

The list of improvements match the adopted Transportation Management Plan and are funded/planned in the 20-year TIP projects.

8.8 Traffic Volumes

Future 2044 traffic forecasts were based on the same method and assumptions as the Sumner Comprehensive Plan 2024. Alternative 1 represents the Sumner Comprehensive Plan as it existed in 2023. The Action Alternative (Alternative 1 and 2) travel forecasts were developed with different housing growth and mix assumptions. The Preferred Alternative included growth in line with City growth targets but with improved housing policies and codes whereas Alternative 1 and 2 studied more housing.

Table 13. Town Center Growth Assumptions

LAND USE BY SUBAREA	PREFERRED ALTERNATIVE		TOTAL GROWTH
	PERMITTED OR IN THE PIPELINE 2020-2023	ADDITIONAL GROWTH	
Housing Units			
Town center in 2024 EIS	307	938	1,245

LAND USE BY SUBAREA	PREFERRED ALTERNATIVE		
	PERMITTED OR IN THE PIPELINE 2020-2023	ADDITIONAL GROWTH	TOTAL GROWTH
Town center plan update 2025	307	938	1,245
Jobs			
Town center in 2024 EIS	32	298	330
Town center plan update 2025	32	398	430

Note: Listed as “Table 12” in adopted Ordinance 2939, table titling corrected for easier navigation

Table 14 provides a summary of the Town Center Trips for the studied Alternatives.

Table 14. Town Center Growth and Trip Assumptions

LAND USE BY SUBAREA	ALTERNATIVE 1 2044	ALTERNATIVE 2 2044	ALTERNATIVE 3 2044	PREFERRED ALTERNATIVE 2044
Housing Units				
Town center	1,245	1,245	1,309	1,245
Jobs				
Town center	330	330	330	330
Trips				
2023: 1,769	2,343	2,594	2,674	2,343

Note: Listed as “Table 13” in adopted Ordinance 2939, table numbering corrected for easier navigation

Additional growth up to 1,985 housing throughout the city (including the 1,245 growth in Town Center), as well as jobs of 5,313 (including 330 jobs in the Town Center Plan) were studied in the Transportation Plan and Comprehensive Plan EIS. The 2025 Town Center Plan Update studied another 100 jobs for 430 in the study area due to the IDEA overlay and found similar results as the studied alternatives and similar operations and improvements needed.

8.9 Traffic Operations

Intersection levels of service were calculated for the weekday PM peak hour as provided in Table 15.

Table 15. Future (2044) Weekday PM Peak Hour Intersection Levels of Service Summary

INTERSECTION	2023 EXISTING			2044 ALTERNATIVE 1 / PREFERRED ALTERNATIVE		
	LOS1	DELAY2	V/C3 OR WM4	LOS	DELAY	V/C OR WM
Traffic Avenue/Main Street	D	43	-	E	58	-
Alder Avenue/Main Street	B	11	-	B	14	-
Valley Avenue/Main Street	C	23	-	C	32	-

Source: Transpo, 2024.

Notes:

1. Level of service, based on 2010 Highway Capacity Manual methodology.
2. Average delay in seconds per vehicle.
3. Volume-to-capacity ratio reported for signalized intersections.
4. Worst movement reported for unsignalized intersections, where SB = southbound, WB = westbound, NBL = northbound left-turn and EB = eastbound.

Note: Listed as "Table 14" in adopted Ordinance 2939, table numbering corrected for easier navigation

Improvements summarized in Section 8.7 and the Transportation Plan are meant to address multimodal levels of service and needs in the city.

8.10 Potential Other Improvements

Mitigation may be needed at intersections to maintain City's adopted LOS D standard, with limited exceptions at constrained Town Center intersections. Additional measures include:

Other measures could include:

- **TDM Strategies** – Employer and residential developer programs (rideshare, transit passes, vanpool/carpool spaces, commuter info, flexible schedules). CTR participation is encouraged to achieve a 5–10% vehicle trip reduction.
- **Non-Motorized Improvements** – Require frontage improvements and complete missing sidewalks and bike connections. Apply LOS standards (orange = primary, green =

secondary) for sidewalks and green LOS for bike routes. Key priorities include the Thompson St–Sounder Station connection and Fryar/Wood trail linkages.

- **Transit Improvements** – Expand Sound Transit coordination, evaluate higher frequency on Route 596, explore Pierce Transit reentry, and assess local circulator shuttle between West Sumner, retail centers, and the Sounder Station.
- **Intelligent Transportation Systems (ITS)** – ITS improvements such as adaptive signal control systems would improve traffic operations at intersections within the Town Center. Implementation of such strategies could result in an overall improvement of 10 to 15 percent.
- Travel demand can be variable and unpredictable, which often outpaces the signal timing plans that are programmed every 3 to 5 years. This can lead to inefficient operation of the signalized intersections resulting in vehicle delays and congestion. Adaptive Signal Control (ASC) seeks to remediate this issue by adjusting signal timing in real-time based on measured vehicle demand. ASC adjusts when green lights start and end to accommodate the current traffic patterns to promote smooth flow and ease traffic congestion. The main benefits of ASC over the conventional time-of-day plans typically include:
 - Automatically adapts to unexpected changes in traffic conditions.
 - Reduces driver complaints and frustration by reducing travel times and increasing arrivals on green.
 - Improves travel time reliability so commute times are consistent throughout the week.
 - Reduces congestion and fuel consumption.
- ASC encompasses the following types of traffic signal control operations:
 - Traffic Responsive – the system monitors detectors along the system corridor and implements pre-configured traffic signal timing plans based on user defined volume or occupancy (congestion) thresholds.
 - Corridor Based Adaptive – the system monitors detectors at each intersection and along the system corridor.
- Capacity Improvements-
 - Wood Avenue/W Main Street – Provision of northbound and southbound left turn lanes would improve intersection operations at this intersection to LOS D during the weekday PM peak hour under future 2035 conditions with

Alternatives 1 and 2. In the 2024 Transportation plan, this intersection shows as a LOS B through 2044 with the current improvements, no additional improvements planned.

- Traffic Avenue/SR 410 WB Ramps/Thompson Street – Provision of northbound left-turn lane and an additional through lane would improve intersection operations at this intersection to LOS D during the weekday PM peak hour under future 2035 conditions with Alternatives 1 and 2. This project was completed in March 2022. In the 2024 Transportation Plan, this intersection shows as a LOS D through 2044
- Traffic Avenue/SR 410 EB Ramps – Provision of an additional northbound left turn lane and eastbound right-turn lane as well as providing a right-turn overlap signal phase would improve intersection operations at this intersection to LOS E during the weekday PM peak hour under future 2035 conditions with Alternatives 1 and 2. This project was completed in March 2022. In the 2024 Transportation Plan, this intersection shows as a LOS C through 2044.
- Traffic Avenue/SR 410 WB Ramps/Thompson Street – Provision of two additional southbound right-turn lanes with a permissive/overlap phasing would improve intersection operations to LOS D during the weekday PM peak hour under future 2035 conditions with Alternatives 1B. Additionally, based on operations, it is anticipated that the second southbound through lane may not be needed and could instead be utilized as one of the southbound right-turn lanes. This project was completed in March 2022. In the 2024 Transportation Plan, this intersection shows as a LOS C through 2044
- Traffic Avenue/SR 410 EB Ramps – Provision of an additional northbound left turn lane and eastbound right-turn lane (for four total eastbound lanes) as well as permissive/overlap phasing for the eastbound right-turn would improve intersection operations at this intersection to LOS E during the weekday PM peak hour under future 2035 conditions with Alternatives 1B. This project was completed in March 2022. In the 2024 Transportation Plan, this intersection shows as a LOS C through 2044.
- Fryar Avenue/Zehnder Street – Providing northbound and westbound right turn lanes as well as an additional left turn (for a total of two) would decrease the delay; however, the intersection would continue to operate at LOS F during the weekday PM peak hour. Adding northbound and southbound through lanes (for a total of two through lanes in each direction) would improve operations to LOS D. In the 2024 Transportation plan, this intersection shows as a LOS C through 2044, no plans for intersection improvements in the 20 year TIP.

8.11 Transit and Rail Service

Sound Transit provides bus service in the City of Sumner. The majority of the routes provide transit service to the Sumner Sounder Train Station facility located on the west side of Traffic Avenue at Maple Street. Based on Sumner 2025 conditions, transit routes that serve the Sumner Train Station include Routes 577/578 and 596. In addition, Pierce County provides Beyond the Borders Connector bus service for eligible residents to public transportation, medical services, employment, shopping, and social activities.

8.12 Commuter Rail

Sound Transit's Sounder line offers commuter rail service between Lakewood and downtown Seattle with stops in Tacoma, Puyallup, Sumner, Auburn, Kent, and Tukwila. Sound Transit's Sounder service shares the Burlington Northern Santa Fe (BNSF) tracks. The Sumner Station is located south of Maple Street at 800 Narrow Street in downtown Sumner. The station opened in September 2000 and was part of the first phase of Sound Transit's program to provide commuter rail service between Everett and Lakewood. During the commute hours there are currently 13 morning and afternoon trains serving the Sumner Station. In addition there are two "reverse commute" trains in the morning and three in the afternoon. There are about 1,500 boardings a day at the Sumner station. According to Sound Transit, there were only 288 total parking spaces at the station. Sound Transit is constructing a 4.5 story parking garage over existing parking lot to be completed by 2026 and will net 550 additional parking spaces to meet demand.

8.13 Bus Service

Route 577/578 provides service between Seattle to Puyallup. This is intended to be a train shadow and currently runs with stops in Puyallup, Sumner, Federal Way, and has three stops in Seattle. The route operates on 30-minute headways on weekdays and hour headways on weekends.

Route 596 provides shuttle service between Bonney Lake Park and Ride to Sumner Sounder Station. The route operates on 20-30 minute headways on weekdays and no weekend service. This route is scheduled in coordination with the train schedule to shuttle commuters to and from the Bonney Lake Park and Ride.

8.14 Beyond the Borders Connector

Pierce County provides a local bus service called Beyond the Borders, which helps eligible residents access public transportation, medical services, employment, shopping, and social activities. There is

no cost to riders. Use of the service is unlimited and riders can get on and off at all stops throughout the community and ride multiple times each day.

8.15 Non-Motorized Transportation

Within the Town Center Plan transportation corridors will focus on multi-modal options with focus on providing streetscapes that include narrower driving isles, parking appropriate for the street, landscaping, sidewalks, bulb-outs, plazas and consideration for future bus stop locations.

Much of the Town Center contains a gridded street pattern with sidewalks on both sides of the street. Sidewalks exist along some of the study area arterials. Where sidewalks are not available, pedestrians must use the roadway shoulders. Arterial and collector roadways that currently have sidewalks within the Town Center include:

- Main Street (Traffic Avenue to 158th Avenue Court East)
- Traffic Avenue (Main Street to Thompson Street)
- Thompson Street (Traffic Avenue to Alder Avenue)
- Alder Avenue (Main Street to Thompson Street)
- Wood Avenue (Southern terminus to Zehnder Street)

Additionally, wider sidewalks are provided along both sides of the local streets in the lower density residential area of the Town Center and in the vicinity of the Sumner Station. Marked crosswalks are also provided at a number of the local streets in the Town Center.

There are limited formal bicycle facilities in Sumner and a few developed in the Town Center, along Academy Street and Wood Avenue. For the most part, bicyclists share the road with motorized traffic or use paved roadway shoulders, where available. Formal bike lanes are present on both sides of Valley Avenue and both sides of Fryar Avenue from Main Street to the Fryar Avenue Bridge, outside of the Town Center Subarea. Zehnder Street will contain bike facilities and wider sidewalks to accommodate pedestrians and cyclists.

8.16 Parking

In 2000, as the Sumner Sounder Train Station was opening, concerns about parking supply and enforcement were voiced by citizens and business owners. This concern still remains as parking has been a significant impact to neighborhood streets. The following have been implemented to help reduce the impact of Sounder Station parking:

- Restricted Parking Zones on most of the streets within a $\frac{1}{4}$ mi. walk of the train station

- Signage of public parking lots
- Lighting and security
- Increased parking restrictions and enforcement
- Allow for publicly-owned paid parking lots

Access to commuter rail platforms and egress from station parking lots were contributing to on street parking demand.

Sound Transit, through funding approved in the ST2 plan, is constructing a 4.5 story parking garage on the existing parking lot at the station and it will net about 550 new spaces. The parking garage is planned to be opened in 2026.

8.17 Future Parking Demands

Future residential and employment growth will bring increased demand for parking. However, state legislation is shifting how cities address parking in urban and transit-rich areas. **HB 1491** encourages transit-oriented development (TOD) near rail stations, supporting higher densities and reducing reliance on automobiles. Similarly, **HB 1337** expands allowances for ADUs and reduces off-street parking requirements, recognizing that excessive parking can constrain housing capacity and affordability. In most instances the developer of these properties will be inclined, for leasing and desirability, to provide a minimum amount of parking necessary to serve their tenants and customers. Typically, especially for TOD where housing is built within close walking distance to transit and residents have less of a need for a car for commuting, there is a reduced demand for parking. Parking requirements for development within the Town Center Plan should be reduced for projects that qualify as transit-oriented development. Residential development within $\frac{1}{4}$ mile of transit typically show a reduced parking demand as residents own fewer cars than those that live in more auto dependent developments (i.e. single-family home, garden-style apartments).

Developments throughout the Puget Sound and Portland Area including neighboring cities of Puyallup, Kent, and Auburn, all with Sounder stations, have reduced parking ratios for residential as illustrated in Table 16.

Table 16. Parking Ratios in Other Cities

CITY	USE	PARKING REQUIREMENT
DuPont	MF	2/unit
Bonney Lake	MF	2/unit
Seattle	MF	1/unit, no minimum for affordable housing projects
Puyallup	CBD-core	1/unit
Puyallup	RM-core	1.5/unit
Auburn	MF	1.5/unit
Auburn	MF within .25 mile of transit station	1.125/unit
Kent	MF	1/unit
Lakewood	MF	1.5-1.75/unit
Sumner	MF	1.25 (studio) – 2.25(2bd<)/unit
Sumner	SFR	2/unit

Note: Listed as “Table 15” in adopted Ordinance 2939, table numbering corrected for easier navigation

The Town Center is envisioned as a walking-friendly environment with closely spaced blocks and a gridded street network that reduces the need for automobile trips. While this multimodal focus will decrease long-term reliance on parking, it is important to recognize that certain uses—such as professional offices—will continue to generate higher parking demand and should provide parking consistent with current zoning standards.

The following is proposed for residential parking within the Town Center Plan area:

Residential Parking Ratios

- 1.25 off-street spaces per housing unit as a baseline requirement.
- Reduced ratio of 0.85 spaces per unit may be allowed when:
 - The site is within $\frac{1}{4}$ mile of the Sumner Sounder Station,
 - On-street parking is available along the property frontage, and
 - A parking study demonstrates that proposed supply will meet market demand.

The following is proposed for commercial parking:

Within the Town Center area, certain new and expanding uses are allowed an administrative waiver from the parking requirements, provided all of the following are satisfied:

Commercial and Office Parking

- Retail, restaurant (<3,000 sf), small assembly (<50 occupancy), or neighborhood food markets (<2,500 sf) may qualify for an administrative waiver of minimum parking, provided:
- The property owner agrees not to protest future formation of a Local Improvement District (LID) to provide shared parking, and
- Existing accessory parking for nearby structures is not displaced.
- Professional office and business uses should supply 2.5 spaces per 1,000 sf of gross floor area, consistent with current zoning requirements.

The demand for future parking is uncertain, but the trend is likely less reliance on the automobile then now or in the past given the introduction of autonomous cars, Millennial generation showing less interest driving and car ownership, increased transit and increased rideshare options such as UBER, LYFT. Given these uncertainties, the City should plan to conduct a parking study every 5 years to evaluate demand, supply, and determine recommendations for future changes. The rate of growth in the Town Center Plan will allow for adjust parking requirements to changing times.

While some level of parking is essential, it should not dominate the urban environment. Surface parking should be minimized and, when necessary, located behind or beside buildings with adequate landscaping or screening along public frontages. Structured parking—either in standalone garages or under-building formats—should be encouraged or required to maintain a continuous, pedestrian-oriented streetscape. Surface parking, should be limited in the Town Center Plan area when possible and structured parking should be encouraged or required. Structured parking, whether in garage structure, or underbuilding, provides the screening and building form at the street edge that is desired and illustrated throughout the Plan.

Recent state legislation has been enacted to encourage affordable housing, infill development, and TOD and resulted in reductions to parking minimums for affordable housing and middle housing options within the station area ($\frac{1}{4}$ – $\frac{1}{2}$ mile from the Sounder Station). The Town Center parking requirements will change as a result of this new legislation.

Recent state laws significantly reshaping local parking policies:

- **HB 1491** promotes TOD near rail stations, enabling higher-density housing with lower parking demand.
- **HB 1337** expands allowances for ADUs and prohibits excessive off-street parking requirements for ADUs, especially near transit.
- **SB 5184 (2025)** caps local parking minimums at 0.5 stalls per multifamily unit and 2 stalls per 1,000 sf commercial, while prohibiting minimums for affordable housing, senior housing, ADUs, small residential units (<1,200 sf), and ground-level mixed-use.
- **RCW 36.01.397** further limits parking requirements for ADUs and certain housing types.

Together, these mandates will require Sumner to phase-down the Town Center parking minimums over time, aligning local code with state law while supporting the Plan's multimodal and housing goals.

The Downtown Parking Study (2001, updated 2006) remains relevant and provides strategies that continue to apply in 2025:

- **Short-term:** expand restricted parking zones near the Sounder Station, review loading zone needs, improve signage, and strengthen enforcement.
- **Medium-term:** replace public parking lost to redevelopment, explore new shared or remote lots, and partner with Sound Transit on future structured parking.
- **Long-term:** adopt stronger demand management tools, enhance both on- and off-street management, and implement long-term solutions for commuter rail parking demand.

The full recommended list of parking strategies are summarized below.

Short-Term Parking Recommendations

- S-1. Establish a parking management review committee
- S-2. Periodically review loading zone requirements
- S-3. Continue delineating time restricted on-street parking spaces
- S-4. Expand Restricted Parking Zone surrounding commuter rail station area
- S-5. Revise RPZ time limits (completed and expanded)
- S-6. Consider RPZ revisions to match demand (revised)
- S-7. Negotiate a parking strategy with Sound Transit (new parking garage planned)
- S-8. Design and install new public parking signage (completed)
- S-9. Continue enforcement and expand as needed. (hired code enforcement officer)
- S-10. Prepare information packet/brochure/flyer regarding various parking options. (completed)

Medium-Term Parking Recommendations

- M-1. Improve existing off-street parking lots (new parking constructed for City Hall)
- M-2. Provide for replacement of public parking spaces when surface parking lots redevelop
- M-3. Establish parking fund
- M-4. Formalize use of Sound Transit commuter rail station parking during large festivals and/or weekend parking
- M-5. Consider sites for additional off-street public parking and revise City code as needed

- M-6. Explore partnership with Sound Transit to locate and construct a new parking facility
- M-7. Establish local circulator buses
- M-8. Seek options for remote parking for carpools

Long-Term Parking Recommendations

- L-1. Require transportation demand management
- L-2. Consider enhanced on-street parking management options
- L-3. Consider enhanced off-street parking management options
- L-4. Develop a long-term solution to excess Sound Transit parking demand in Sumner

8.18 Local Transit System

In order to provide further connectivity between the Historic Downtown and the newer developing eastern end of Main Street, and with the East Sumner Neighborhood Planned area, the City is considering developing a local transit system. The local transit system could be small vans or buses or autonomous electric shuttles. The local transit system could circle the West Sumner District, stop at the Sounder Station and continue down Main Street to Valley Avenue to the YMCA, and possibly WinCo Food and other businesses on the east end of town. The main purpose of the local transit system would be to shuttle residents and visitors between the commercial business areas and provide yet another mode of transportation for all ages and abilities. The presence of a local transit system for errands would likely decrease the demand for auto ownership in the Town Center.

The City would need to explore the feasibility of a local transit system. Currently, Sumner is not within the Pierce Transit service area. The City could consider the options contracting for services, annexing back into Pierce Transit service area or provide the service themselves.

9 Public Utilities

The City of Sumner's Capital Facilities Plan (CFP) is a supporting document to the Comprehensive Plan. The CFP identifies what public facilities and services are needed for the planned growth and the Capital Improvement Program (CIP) identifies how these improvements are financed and developed. The following utilities serve the Town Center Plan area.

9.1 Water

The City's Water System Plan shows that the historic City center is well served by the existing water lines; however, upgrades to the size of some lines will be necessary to accommodate the planned development. The City's Comprehensive Water System Plan anticipates growth to 2068 and identifies that planned growth to 2044 could be accommodated through regular monitoring and capital planning, as the Water System Plan is updated every 6-10 years. Identified in the 2020 CIP improvements for water facilities related to the distribution system include source improvements, storage, and operations and maintenance to improve the system and accommodate growth. The overall water capacity for the City can accommodate future growth in the Town Center that is expected as the overall population targets for the City remain consistent with Pierce County density targets.

9.2 Sanitary Sewer

The Town Center Planning area is served by gravity sewer lines which eventually lead to the City's Wastewater Treatment Plant at the confluence of the White and Puyallup Rivers. The Plan Area is located in two sewer basins, Wastewater Treatment Plant Basin and South Pump Station Basin. Identified deficiencies include periodic repetitive maintenance at Fryar Avenue north of Main Street (grease accumulation), and Sumner Avenue north of North Street (pipe cleaning once a week). The City's Comprehensive Sanitary Sewer Plan identifies existing gravity sewer deficiencies, though there are no pump station or force main deficiencies. The 2020 CIP for sanitary sewer shows a number of projects in the 20-year planning period (2018-2038) to address system deficiencies and accommodate growth including sewer line extension, replacement, and relocation; wastewater treatment upgrades; pump and equipment replacement. Population and job growth to 2044 generally aligns with the projected growth rate through 2038 in the City's Comprehensive Sanitary Sewer Plan.

9.3 Stormwater

The Town Center Planning Area has a complete storm drainage system consisting of tight-lined pipe with collection and direct discharge to the rivers. System maintenance is the primary utility activity in this area. During extreme storm events, the shallow slopes in town preclude rapid draining of water. Future development and redevelopment will comply with the City's stormwater management regulations. The City adopted the 2018 Department of Ecology Stormwater Management Manual for Western Washington. The Manual contains requirements for treatment of stormwater run-off and for detention/retention and low-impact development standards. The provision of stormwater management structures is required when new development:

Creates or adds 2,000 square feet, or greater, of new, replaced or new plus replaced impervious surface area, or has land disturbing activity of 7,000 square feet or greater.

These requirements are more restrictive than the current regulations. However, much of the Town Center Area already consists of impervious surfaces except along the White (Stuck) River.

10 Private Utilities

10.1 Telecommunications

Comcast provides basic telephone service. Its central switching station is located in the Town Center Planning Area at Maple Street and Kincaid Avenue. Main and branch feeder routes are found along Main Street, Alder Avenue and Wood Avenue. Sprint provides a fiber optic line running along Traffic Avenue and along the BNSF Railroad right-of-way, with a major spur branching off to Tacoma.

10.2 Gas and Electric

Puget Sound Energy provides natural gas and electric service to the Town Center Planning Area. Electricity is available throughout as a basic service. A major electric transmission line follows the BNSF Railroad right-of-way. In 2016 Puget Sound Energy installed a large 230 kilovolt (KV) powerline with 120 foot power poles along the southwestern and western edge of the Town Center Plan area. These lines cross the Puyallup River just south of Puyallup River Bridge and traverse the lower southwest quadrant of the West Sumner District before crossing the White River and following the Union Pacific Railroad tracks north.

Natural gas is supplied based upon customer request. PSE's current planned projects for the City of Sumner area include (subject to on-going review and revision as additional load and growth information is received) the following for the Town Center area:

- Replace 1" natural gas main with 2" main along North Street from Ryan Avenue to Sumner Avenue.
- Replace 4" natural gas main with 8" main along Wood Avenue from Main Street to Zehnder Street.

II Implementation

II.1 Implementation Steps

Implementation of the Sumner Town Center Plan will require amendments to the Sumner Comprehensive Plan goals and policies in various elements and to the Comprehensive Plan Land Use Map. These amendments will be made concurrent with final adoption of the Town Center Plan and done by separate ordinance and City Council decisions.

Implementation of the Sumner Town Center Plan also requires adoption of a form based code and amendments to the Zoning Code, Zoning Map, Design and Development Guidelines, these amendments will be made concurrent with the adoption of the Town Center Plan.

II.2 Planned Action Ordinance

The State Environmental Policy Act (SEPA) and implementing regulations provide for the integration of environmental review with land use planning and project review through the designation of "planned actions" by jurisdictions planning under the GMA.

The City Council adopted a Planned Action Ordinance per the SEPA (RCW 43.21C.440 and WAC 197-11-164 through 172) concurrent with adoption of the Town Center Plan. The designation of a planned action expedites the permitting process for projects of which the impacts have been previously addressed in a supplemental environmental impact statement (SEIS). The Planned Action Ordinance No. 2941 was adopted November 2025 to reflect changes in projected population and employment growth.

II.3 Shoreline Master Program

The City of Sumner Shoreline Master Program (SMP) is mandated and approved by the Department of Ecology and implements the state Shoreline Management Act (SMA) per RCW 90.58 and WAC 173-26. The SMA was intended to protect and restore the valuable natural resources that the state's shorelines represent. In addition, the SMA was developed to plan for and foster all "reasonable and appropriate uses" that are dependent upon a waterfront location, or which will offer opportunities for the public to enjoy the state shorelines.

The current Shoreline Master Program limits building heights along the White River to 35 feet maximum. This height limit will need to be increased to accommodate the 5-6 story structures proposed in the Town Center Plan.

These amendments were a part of the periodic review of the SMP in 2020.

12 References

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The Transpo Group (September 2018). Sumner Town Center Subarea Plan Update Transportation Study.

The Transpo Group (January 2025). City of Sumner Transportation Plan

13 Appendices

- A. Summary of 2018 Planning Process & Forums for the 2018 Town Center update
- B. Public Participation Plan for the 2025 Town Center update
- C. Ordinance No. 2939 Adopting 2025 Town Center Plan Update and Amending the Comprehensive Plan

Appendix (A)- Summary of 2018 Planning Process & Forums for the 2018 Town Center update

Each of the studies and forums contributed to the 2018 Town Center Plan update and are summarized below.

2018 Studies

- Real Estate and Retail Market Study:** EcoNorthwest, September 2018. Prepared to analyze the potential for multi-family mid-rise urban style development and potential on specific opportunity sites.
- 2015 Supplemental Environmental Impact Statement:** BERK Consulting. September 2018. This document was scoped to address topics pertinent to this project including aesthetics and transportation analysis.
- Planned Action Ordinance (PAO):** BERK Consulting. September 2018. A PAO is important to the overall desire to facilitate development in the Town Center Plan Area.
- Sumner Town Center Subarea Plan Update Transportation Study:** Transpo Group. July 2018. An analysis of the transportation impacts associated with increased housing and vehicle trips within the Plan area.

2018 Forums

- Updated Town Center Plan Vision, August 2017.** Planning staff lead a bus tour with the Mayor, City Council, Planning Commission, and Design Commission and key staff members to Issaquah Highlands and Auburn to experience greater heights and densities that could be possible in Sumner and obtain needed feedback.
- Town Center Plan Kick Off Meeting, October 5, 2017.** This public forum presented visual preferences on different multi-family styles and types, streetscapes, and single-family developments. Participants included citizens and business owners who gathered in small groups and focused on different topics including Business Growth and Economics, Housing Development, Parks and Open Space and Transportation. Input received included visual preference voting and colored maps from the attendants on future development character and height.
- Town Center Plan Alternatives Open House, February 15, 2018.** In this public forum there were three alternatives illustrating different land use and height options. There was opportunity for open discussion at each board with planning staff seeking public feedback and comments. The comments and recommendations helped shape the preferred alternative map for the Town Center Plan.
- Development Community Interviews, August 2018.** As a component of public outreach plan City staff met with members of the development community including developers, real estate brokers, architects, and civil engineers regarding the proposed Town Center Plan and received comments and direction on the real estate market, incentives, and parking options.

SUMNER COMPREHENSIVE PLAN

2025 Annual Amendments-Public Participation Plan (PPP)

During the 2025 update of the Comprehensive Plan and Town Center Plan, staff propose to engage and inform the public of the proposals through the following outreach efforts. The Planning Commission approved this PPP on March 6, 2025.

- ❖ Posting updated information throughout process:
 - City website notices
 - Sumner Connects project page: <https://connects.sumnerwa.gov/planning-sumners-future>
- Updates include:
 1. Regular updates to project “life cycle”
 2. Planning Commission meeting packets uploaded
 3. Include links to public notices
 4. Update “What’s Next” text to keep public informed
 - City weekly e-news
 - City quarterly newsletter (June, September)
 - City’s social media pages
 - Informational flyers and boards presented at Community Open Houses
- ❖ **Public Notices:** Will be posted in the Newspaper, City newsletter, City website, Constant Contact emails to Public Notice List, on-site posting of affected properties, direct mailing to affected properties. Additionally, every property in the Town Center Plan area received a direct mailer for Notice of Applications and Notice of Public Hearing on MA-01. Below is the list of the public notices as of September 25, 2025:
 1. Notice to open the annual amendment cycle: January 23, 2025
 2. Notice of Comprehensive Plan Amendment applications received: April 10, 2025
 3. Notice of SEPA Determination:
 - a. MA-01 SEPA decision issued on: August 20, 2025
 - b. MA-02 SEPA decision issued on: August 7, 2025
 - c. MA-03 SEPA decision issued on: August 18, 2025
 - d. TA-04 SEPA decision issued on: August 7, 2025
 4. Notice of Public Hearing with Planning Commission: September 28, 2025
 5. Future Public Hearing notice with Council (end of October)
- ❖ **Comment periods for environmental reviews:** Public posting of Notice of Decisions for each proposal provides 15-day comment period.

❖ Town Center Plan outreach:

- Alternatives & maps presented at Open House April 2025
- Alternatives & maps posted Sumner Connects
- Mailing to affected property owners + neighbors within 500 feet
- Focus group meeting with residents and stakeholders June 2025
 1. Public Notice: Mailed to all affected property owners
 2. Focus Group Meeting advertised in the City Newsletter
 3. Direct invites to Puyallup/Sumner Chamber, Commercial/Industrial real estate brokers, Local small-scale business innovators
- Informational meeting with Sumner Main Street Association (SMSA) Economic Vitality Committee August 2025

❖ **Community Open House:** Present alternatives & information with displays, questions and surveys for attendees at the April 2025 & September 2025 open house.

Appendix (C)- Ord 2939

Appendix C: Ordinance No. 2939 Adopting 2025 Town Center Plan Update

ORDINANCE NO. 2939
CITY OF SUMNER, WASHINGTON

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF SUMNER, WASHINGTON,
ADOPTING AN UPDATED TOWN CENTER PLAN AND AMENDMENTS TO THE
COMPREHENSIVE PLAN.**

WHEREAS, the City of Sumner's Comprehensive Plan was originally adopted by Ordinance No. 1625 on April 4, 1994 to comply with the Growth Management Act, addressing land use, community character, transportation, regional planning, the environment, open-space and parks, capital facilities and utilities; and

WHEREAS, the Washington State Growth Management Act, per RCW 36.70A.130 and the Sumner Municipal Code 18.56.147 allows for annual amendments of the Comprehensive Plan, development regulations, and other supporting plans and documents; and

WHEREAS, the City considered the following amendments to the Comprehensive Plan and made up the annual amendments docket for 2025: 1) MA-01- Comprehensive Plan Amendment - Town Center Plan Update; 2) MA-02 – Southeast Urban Growth Area (SE UGA) Redesignation from LDR-2 to MDR and IC; 3) MA-03 – 30th Street East Redesignation/Rezone from LDR-2/LDR-8.5 to M-1; and 4) TA-04-Text Amendment- Sumner-Bonney Lake School District Capital Facilities Plan and update to the City of Sumner Capital Facilities Plan; and

WHEREAS, the City of Sumner established a public participation plan pursuant to RCW 36.70A.130(2)(a) and provided notice to all persons requesting notice of a comprehensive plan amendment update and provided opportunity to comment; and

WHEREAS, the City held a duly noticed public hearing with the Planning Commission on September 11, 2025, and several public meetings including April 15, 2025, June 10, 2025, and September 30, 2025; and Council public hearing on November 3, 2025; and

WHEREAS, in compliance with the State Environmental Policy Act (SEPA) RCW 43.21C and WAC 197-11, the City of Sumner issued a Mitigated Determination of Non-Significance (MDNS) on the Town Center Plan Update (MA-01) proposal on August 20, 2025; and

WHEREAS, the proposed Town Center Plan update and MDNS were sent to the Washington State Department of Commerce on September 9, 2025 per RCW 36.70A.106 for a 60-day review; and

WHEREAS, the Town Center Plan Update: 1) creates an IDEA Overlay on in the West Sumner District in the vicinity of Zehnder Street and 57th Street; 2) reduces allowed story heights in the Historic Central Business District (CBD) from 5 stories to 3-4 stories; 3) updates road types, public realm; illustrations, and various other minor amendments; and 4) updates the Planned Action Ordinance as allowed under the State Environmental Policy Act (SEPA) RCW 43.21C.440; and amends the Town Center Code and "form-based code"; and

WHEREAS, on October 2, 2025 a majority of the Planning Commission approved a recommendation that the City Council adopt the updated Town Center Plan Amendments; and

WHEREAS, the City Council has found the updated Town Center Plan amendments to be consistent with the Growth Management Act, VISION 2050 multi-county planning policies, and Pierce County Countywide Planning Policies; and

WHEREAS, the City Council held a duly advertised public hearing on November 3, 2025 and held a prior study session on October 13, 2025, to deliberate and discuss alternatives; and

WHEREAS, the updated Town Center Plan amendments are consistent with the goals and policies of the Comprehensive Plan.

**NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF SUMNER, WASHINGTON
DO ORDAIN AS FOLLOWS:**

Section 1. That the revised Town Center Plan is adopted to read as shown in Exhibit A, which is attached hereto and incorporated by reference.

Section 2. That the Sumner Comprehensive Plan section 1, “Introduction,” subsection “Growth Management Act,” is amended to read as follows:

[. . .]

“The City has chosen, over the years, to implement the GMA in a number of ways with additional planning and guidance as follows:

- 1994—Community Character Strategy
- 1996—Design and Development Guidelines
- 2001—East Sumner Neighborhood Plan
- 2015—East Sumner Neighborhood Plan Update
- 2018—Sumner -Pacific Manufacturing Industrial Center Subarea Plan
- 2002—East Main Street Design Strategy
- 2018—Town Center Subarea Plan
- 2020—East Sumner Neighborhood Plan Update
- 2021—Main Street Visioning Plan
- 2021—Housing Action Plan
- “2025- Town Center Plan Update”

Section 3. That the Sumner Comprehensive Plan section 1, “Introduction,” subsection “Sumner’s Comprehensive Plan History” is amended to read as follows:

[. . .]

“2018 Amendments-Updated Town Center Subarea Plan Sumner-Pacific
Manufacturing/Industrial Center Subarea Plan”

Ordinance No. 2652 adopted August 6, 2018 amended the Comprehensive Plan as it related to the Sumner-Pacific Manufacturing/Industrial Center. The amendments created a Sumner-Pacific Manufacturing/Industrial Center Subarea Plan with goals and policies that ensured a coordinated approach to development, environmental review, and strategic capital investments in the Sumner-Pacific Manufacturing/Industrial Center. A Determination of Non-significance for this amendment was issued on this proposal on June 6, 2018 in compliance with the State Environmental Policy Act.

2018 Amendments—Updated Town Center Subarea Plan

Ordinance No. 2666, adopted December 3, 2018, adopted an updated Town Center Subarea Plan and amended the Comprehensive Plan. The updated Town Center Subarea Plan includes amendments to the Comprehensive Plan Map designations and Zoning districts, as well as refreshed policies, strategies, and a form-based code. A Planned Action Ordinance was also adopted pursuant to RCW 43.21C.440 following completion of a Supplemental Environmental Impact Statement.

Section 4. That a new paragraph is added to the Sumner Comprehensive Plan section 1, “Introduction,” subsection “Sumner’s Comprehensive Plan History” to read as follows:

[. . .]

“2025 Amendments—Updated Town Center Subarea Plan

Ordinance No. 2939, adopted January 01, 2026, adopted an update to the Town Center Subarea Plan and amended the Comprehensive Plan. The updated Town Center Subarea Plan includes amendments to the Comprehensive Plan, amendments to the Town Center Zoning district map with the addition of the IDEA overlay, as well as refreshed policies, strategies, and an update to the form-based code. An amendment to the Planned Action Ordinance was also adopted under Ordinance No. 2941 pursuant to RCW 43.21C.440 following completion of Environmental review and Notice to Adopt Supplemental Environmental Impact Statement.”

Section 5. That the Sumner Comprehensive Plan section 1, “Introduction,” subsection “Implementing Plans and Strategies” is amended to read as follows:

[. . .]

“Town Center Plan, 20252018. The update to the 2025 subarea plan was completed by the City with assistance from consultant team BERK, and The Transpo Group and ECONW building off of prior efforts in 2005 and 2018. This document provides the policy and real estate market analysis for revitalizing the downtown core and encouraging development of more housing to serve local businesses and utilize the Sounder station. The update included adoption and amendment of a Planned Action Ordinance per RCW 43.21C.440 following completion of a Supplemental Environmental Impact Statement and subsequent SEPA documents.”

Section 6. That the Sumner Comprehensive Plan section 4, “Plan Monitoring and Amendment,” subsection, “Introduction,” is amended to read as follows:

“The Sumner Vision Statement mentions a number of measures that relate to governance and the role of City government in the community. The community vision anticipates a government that is representative, open, and responsible; and one that is fair and accessible to all segments of the community. The policies in this chapter are not exhaustive but attempt to address the role of City government in the community and in maintaining permitting and enforcement measures that support residents, employees, and business owners. The policies in this chapter outline the framework for monitoring implementation actions, evaluating key performance indicators, and establishing procedures for periodic updates and formal amendments in compliance with state laws and local policies.”

Section 7. That the Sumner Comprehensive Plan section 8, “Economic Development and Employment,” subsection “Goals, Policies, and Objectives” is amended to read as follows:

[. . .]

- "6.15 Encourage more mixed commercial/service/housing uses in the Town Center, along Traffic and Fryar Avenues. ~~Discourage Limit~~ light industrial ~~zoning~~ uses in the Town Center in favor of mixed-use ~~zoning~~ unless there is an educational or artist component."
- "6.16 “Create a market and tourism strategy to attract new business compatible with long-range plan and zoning intents, and strengthen and increase the variety of commercial shopping and service opportunities for the community. Allocate resources to implement market and tourism strategy.”
- "6.17 “Coordinate with downtown business association(s) and Chamber of Commerce on education, e.g. business management, business promotion ideas, entrepreneurship and explore funding opportunities to incentivize incubator businesses. Partner with local agencies and schools on makerspace development.”

Section 8. That a new subsection is added to the Sumner Comprehensive Plan section 8, “Economic Development and Employment,” subsection “Goals, Policies, and Objectives” to read as follows:

[. . .]

- "6.24 Establish an IDEA Overlay zone in the Town Center along Zehnder Street and 57th Street, to encourage mixed-uses comprised of retail, residential, small-scale production, civic institutions and artisan enterprises.”

Section 9. That the Sumner Comprehensive Plan section 14, “Housing,” subsection “Goals, Policies, and Objectives” is amended to read as follows:

[. . .]

- "4.1 Promote the construction of housing stock in the Town Center by at least 500-1000 dwelling units by ~~204435~~.
- 4.2. Adopt greater height limit, 5-6 stories, for development at higher density in West Sumner and west of the White River to promote re-development of the area for transit-oriented development.
- 4.3. Adopt greater height limits, 4-5 stories, east of the BNSF Railroad with a transition area to low density residential at a maximum of 3 stories as appropriate.
- 4.4. Maintain the Town Center “transition areas” that will be at reduced height (maximum 3 stories) and prohibit certain commercial uses to create a more compatible transition between low density residential zones and areas with increased height.
- 4.5. Encourage land assembly allowing for feasible and attractive housing or mixed housing/commercial developments. Waive permit fees associated with lot consolidation such as lot line adjustments. Facilitate matching compatible owners that can work jointly to consolidate and sell/develop.
- 4.6. Prohibit garden style apartments in the Town Center that would promote surface parking.

4.7 In order to promote redevelopment to desired densities, prohibit new single-family residential development and limit expansion, except for the addition of accessory dwelling units of existing single family.”

Section 10. Corrections by City Clerk or Code Reviser. Upon approval of the City Attorney, the City Clerk and the Code Reviser are authorized to make the necessary corrections to this ordinance, including the correction of clerical errors; ordinance, section, or subsection numbering; or references to other local, state, or federal laws, codes, ordinances, or regulations.

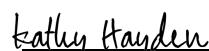
Section 11. Severability. If a section, subsection, paragraph, sentence, clause, or phrase of this ordinance is declared unconstitutional or invalid for any reason by any court of competent jurisdiction; such decision shall not affect the validity of the remaining portions of this ordinance. If the provisions of this ordinance are found to be inconsistent with other provisions of the Sumner Municipal Code, the provisions of this ordinance shall control.

Section 12. Effective Date. This ordinance shall be in full force and effect on January 1, 2026.

Section 13. Implementation. The Mayor is hereby authorized to implement such administrative procedures as may be necessary to carry out the directions of this legislation.

Passed by the City Council and approved by the Mayor of the City of Sumner, Washington, at a regular meeting thereof this 24th day of November, 2025.

Signed by:

kathy Hayden

31045725245
Mayor Kathy Hayden

ATTEST:

DocuSigned by:

Michelle Converse

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City Clerk Michelle Converse, CMC

APPROVED AS TO FORM:

DocuSigned by:

Andrea Marquez

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City Attorney Andrea Marquez

First Reading: November 3, 2025

Date Adopted: November 24, 2025

Date of Publication: November 26, 2025/December 3, 2025

Effective Date: January 1, 2026