

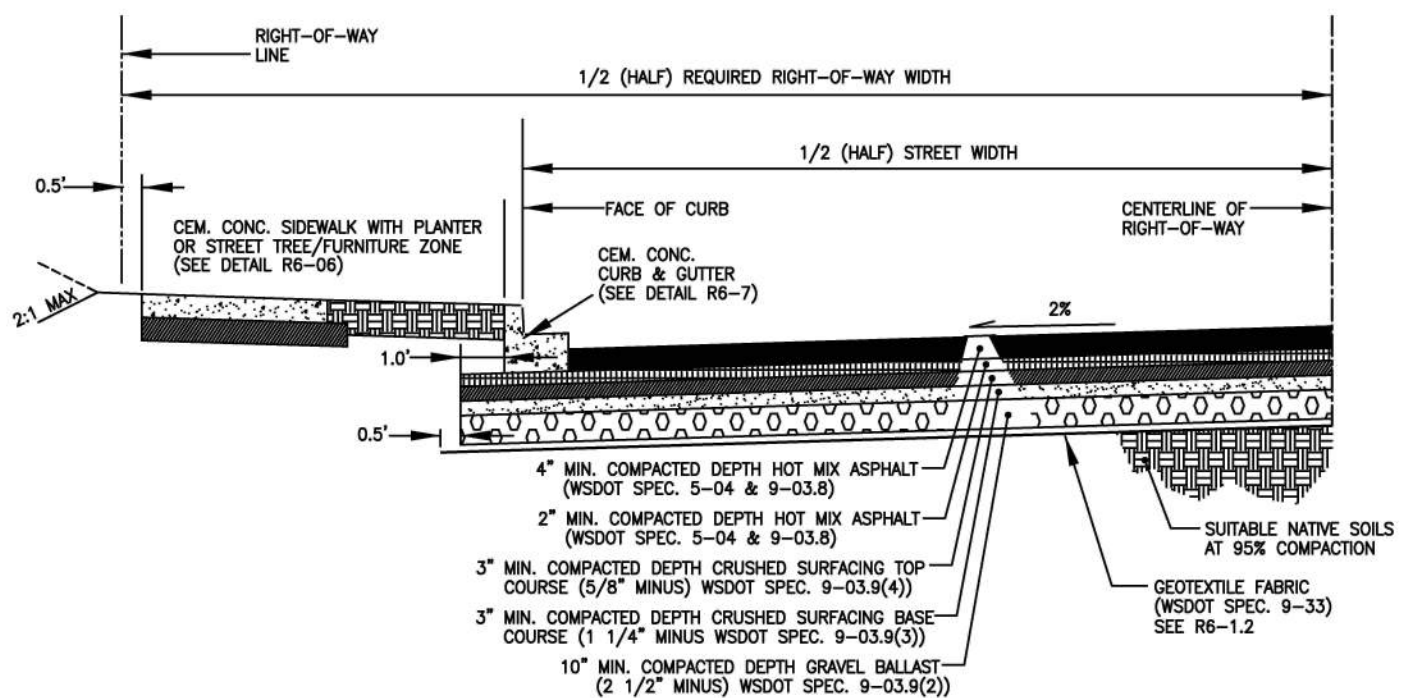
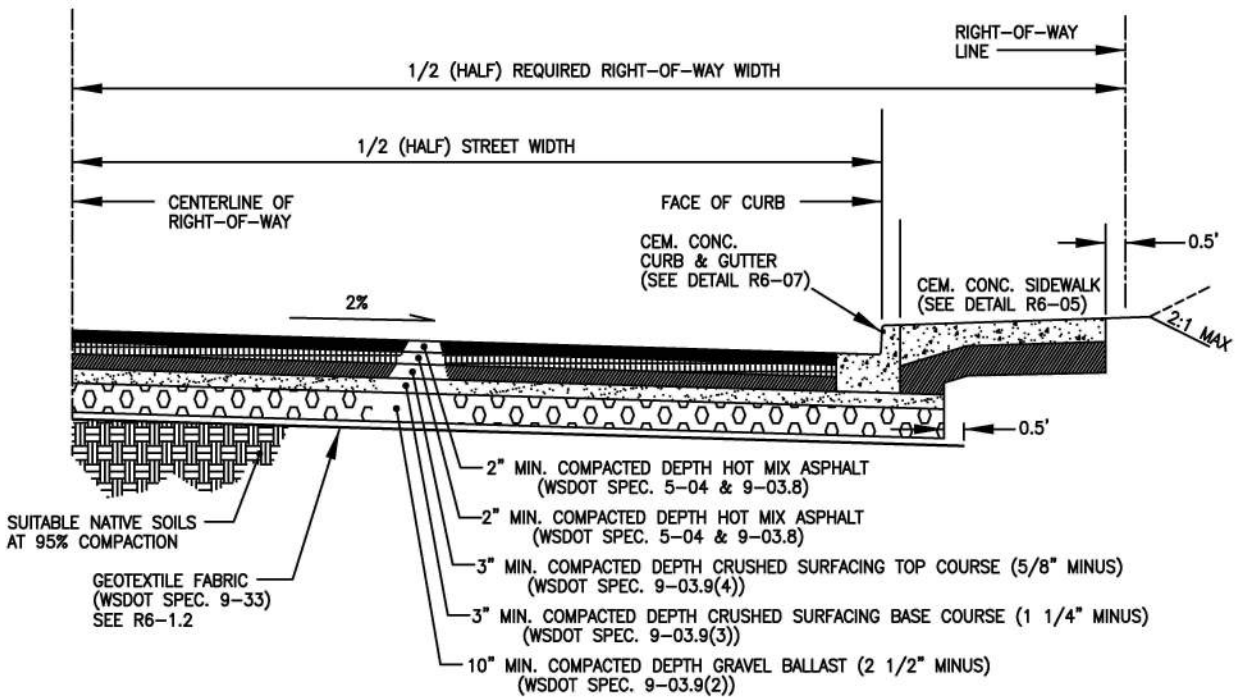
City of Sumner

Public Works Department

STREET

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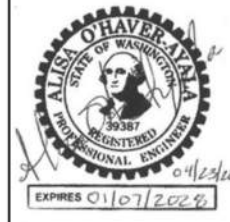
| | TYPE OF STREET | CENTERLINE GRADES MIN. | MIN. ROW WIDTHS | STREET WIDTH MIN. | MIN. PAVEMENT THICKNESS |
|-------------|----------------------|------------------------|-----------------|-------------------|-------------------------|
| RESIDENTIAL | NEIGHBORHOOD STREETS | 10% | 50' | 28' | 4" |
| | RESIDENTIAL STREETS | 10% | 60' | 34' | 4" |
| | COLLECTOR | 10% | 60' | 36' | 6" |
| | MINOR ARTERIALS | 8% | 60' | 38' | 8" * |
| | PRINCIPAL ARTERIALS | 5% | 66'/82' | 40'/60' | 8" * |

| | TYPE OF STREET | CENTERLINE GRADES MIN. | MIN. ROW WIDTHS | STREET WIDTH MIN. | MIN. PAVEMENT THICKNESS |
|------------|-----------------------|------------------------|-----------------|-------------------|-------------------------|
| INDUSTRIAL | LOCAL NON-RESIDENTIAL | 10% | 60' | 36' | 6" |
| | COLLECTOR | 10% | 60'/72' | 36'/50' | 6" |
| | MINOR ARTERIALS | 8% | 60'/72' | 38'/50' | 8" * |
| | PRINCIPAL ARTERIALS | 5% | 66'/82' | 40'/60' | 8" * |

MINIMUM CENTERLINE AND FLOWLINE GRADE FOR ALL STREETS IS 0.5% MAXIMUM AS SHOWN

* SEE NOTE 17 ON STANDARD DETAIL R6-01 (2 OF 2)

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ROADWAY CROSS SECTION



LAST REVISION: 04/01/26

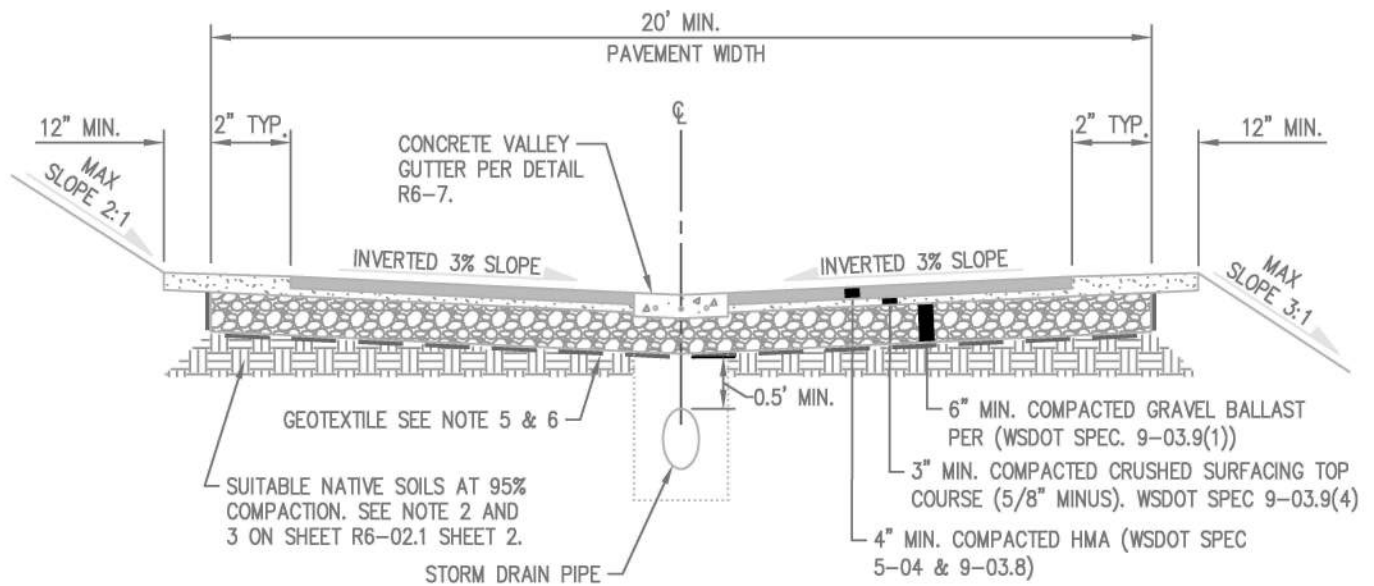
STREET STANDARD DETAIL R6-01

SHEET 1 of 2 N.T.S.

NOTES:

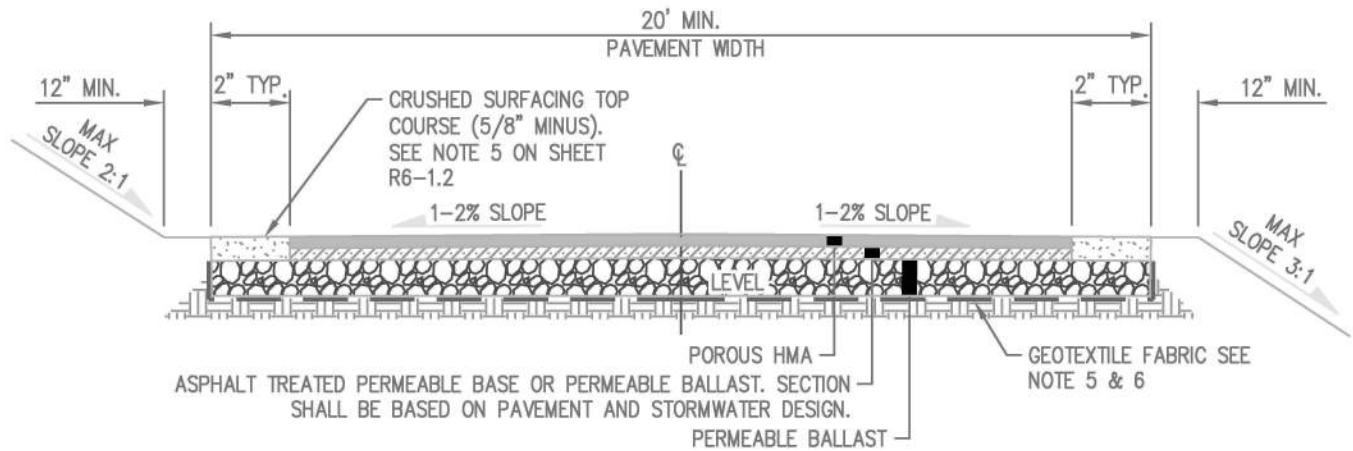
1. ALL DEPTHS ARE MINIMUM COMPACTED DEPTHS.
2. SUB GRADE PREPARATION SHALL MEET THE REQUIREMENTS OF WSDOT SPEC. SECTION 2-06.3(1). THE UPPER ONE (1) FOOT OF THE SUB GRADE SOILS SHALL BE COMPACTED TO AT LEAST 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MEET THE COMPACTION REQUIREMENTS AND CONTROL ALL WORK. THE CITY OF SUMNER REQUIRES COMPLIANCE TESTS, AT THE CONTRACTORS EXPENSE.
3. UNSUITABLE SUBBASE MATERIAL SHALL BE REPLACED WITH FOUNDATION MATERIAL CLASS A MEETING THE REQUIREMENTS OF WSDOT SPEC. SECTION 9-03.17. THE CONTRACTOR SHALL SELECT THE SOURCE , BUT THE SOURCE AND THE QUALITY OF THE MATERIAL SHALL BE APPROVED BY THE CITY ENGINEER.
4. UNLESS A VARIANCE IS GRANTED BY THE CITY ENGINEER, THE CONTRACTOR SHALL PLACE A GEOTEXTILE FABRIC OVER THE EXISTING SUBGRADE. THE SUBGRADE SHALL BE PREPARED AS OUTLINED IN DIVISION 2 OF THE WSDOT SPEC. BEFORE THE PLACING OF THE FABRIC ON THE SUBGRADE. THE CONTRACTOR SHALL PLACE A GEOTEXTILE FABRIC OVER THE PREPARED SUBGRADE WITH A 2 FOOT MINIMUM OVERLAP. THE MATERIAL SHALL BE MIRAFI 500X WOVEN SLIT FILM GEOTEXTILE FABRIC OR AN APPROVED EQUAL (WSDOT SPEC. 9-33.2(1), TABLE 3). THE MATERIAL SHALL BE PROPERLY PACKED AND DELIVERED TO THE SITE AS RECOMENDED BY THE MANUFACTURER. THE CONTRACTOR SHALL NOT BE ALLOWED TO USE ANY PORTION OF A USED ROLL OF FABRIC.
5. CRUSHED SURFACING TOP COURSE BASE MATERIAL SHALL BE COMPACTED TO 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY. CRUSHED SURFACING TOP COURSE SHALL MEET THE REQUIREMENTS AS OUTLINED IN SECTION 4-04 THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL FURNISH AND PLACE THE CRUSHED SURFACING IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AS SET FORTH ABOVE. THE CONTRACTOR SHALL SELECT THE SOURCE, BUT THE SOURCE AND QUALITY OF THE MATERIAL SHALL BE APPROVED BY THE CITY ENGINEER.
6. ASPHALT CONCRETE PAVEMENT SHALL BE INSTALLED IN ACCORANCE WITH WSDOT SPEC. SECTION 5-04.
7. THE CITY RESERVES THE RIGHT TO MODIFY THE GRADATION OF THE ASPHALT CONCRETE PAVEMENT IF THE CONDITIONS WARRANT IT. THE CONTRACTOR SHALL SUBMIT A JOB MIX FORMULA TO BE REVIEWED BY THE CITY ENGINEER PRIOR TO ANY ASPHALT PLACEMENT.
8. TEMPERATURE SHALL NOT EXCEED 325° F AT DISCHARGE OF THE PLANT NOR LESS THAN 190° F LEAVING THE SPREADER BOX.
9. THE MAXIMUM COMPACTED THICKNESS OF ANY SINGLE LIFT OF PAVEMENT SHALL BE 3". PAVEMENT SECTIONS OF THICKNESS GREATER THAN 3" SHALL BE PLACED IN LIFTS. EACH LIFT SHALL BE COMPACTED IN ACCORDANCE WITH WSDOT SPEC. SECTION 5-04.3(10) TO A MINIMUM AVERAGE COMPACTED DENSITY OF 92% OF THE MAXIMUM REFERENCE DENSITY AS DETERMINED BY WSDOT TEST METHOD 705. PERIODIC COMPLIANCE TESTS SHALL BE MADE AT THE EXPENSE OF THE CONTRACTOR.
10. THE FACE OF THE GUTTER LIP AND EDGES OF EXISTING ASPHALT MEET LINES SHALL BE TACK COATED PRIOR TO PAVEMENT PLACEMENT. WHEN SUCCESSIVE LIFTS OF ASPHALT ARE REQUIRED, TACK COAT SHALL BE DISTRIBUTED UNIFORMLY OVER THE PREVIOUS LIFT PER WSDOT SECTION 5-04.3(5) AND SHALL BE ALLOWED TO SET TO A TACKY STATE PRIOR TO THE PLACEMENT OF THE NEXT LIFT.
11. ALL MEETLINES BETWEEN LIFTS OF ASPHALT SHALL BE UNIFORM, WITH THE EDGES VERTICAL AND AT THE REQUIRED THICKNESS. IF SUBSEQUENT LIFTS ARE NOT COMPLETED WITHIN 48 HOURS OR THE EDGES HAVE BEEN CONTAMINATED, THE MEET LINES SHALL BE CLEANED AND TACK COATED.
12. ALL EXISTING MANHOLE COVERS AND MONUMENT CASES SHALL BE REMOVED AND STORED. THE MANHOLES AND MONUMENTS SHALL BE COVERED TO PREVENT DIRT AND DEBRIS FROM ENTERING DURING PAVING OPERATION. AFTER PAVING, THE CASTINGS SHALL RE-INSTALLED TO THE PROPER ELEVATION AND PATCHED IN ACCORDANCE WITH THE CITY OF SUMNER SPECS.
13. MONUMENTS SHALL NOT BE REMOVED BY THE CONTRACTOR UNTIL PROPERLY REFERENCED BY A LICENSED LAND SURVEYOR. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO HAVE MONUMENTS REFERENCED AND REPLACED BY A LICENSED LAND SURVEYOR PER THE SURVEY RECORDING ACT, SECTION RCW 58.09.130 AND WAC 332-120. NO PROJECT APPROVALS SHALL BE GIVEN UNTIL A COPY OF THE DNR GRANTED PERMIT FOR TEMPORARY MONUMENT REMOVAL OR POTENTIAL DISRUPTION IS RECEIVED BY THE CITY OF SUMNER PUBLIC WORKS DEPARTMENT.
14. ANY CHANGES TO THE STANDARD PAVEMENT SECTION SHALL REQUIRE APPROVAL OF THE CITY ENGINEER. A STRUCTURAL PAVEMENT CROSS SECTION DESIGN WITH CALCULATIONS SHALL BE REQUIRED.
15. ALL MANHOLE FRAMES, VALVE BOXES AND MONUMENT COVERS SHALL BE INSTALLED AFTER FINAL LIFT OF ASPHALT PAVEMENTS. SEE SPECIFIC DETAILS FOR METHOD OF INSTALLATION. CATCH BASIN AND MANHOLE FRAMES AND COVERS TO BE UPGRADED TO NEW STANDARDS BY THE CONTRACTOR.
16. CITY ENGINEER MAY DETERMINE ADDITIONAL SUBBASE MATERIAL DEPTH REQUIRED UPON AN ENGINEERED SOIL ANALYSIS.
17. IF PAVEMENT SECTION IS GREATER THAN 6" FIRST LIFT (2" MIN) SHALL BE PLACED PRIOR TO AND UNDER CONCRETE CURB AND GUTTER.
18. REFERENCE SPECIFIC SUB-AREA PLANS, SUCH AS THE TOWN CENTER PLAN, FOR SPECIFIC ROADWAY CROSS SECTION DIMENSIONS.

| | | | |
|--|--|---|--|
| APPROVED BY CITY ENGINEER | |  | |
|  | | ROADWAY CROSS SECTION - NOTES | |
| LAST REVISION: 04/01/26 | | STREET STANDARD DETAIL | |
| SHEET 2 of 2 | | R6-01 | |



HMA ALLEY WITH VALLEY GUTTER

| MATERIAL DEPTH | RESIDENTIAL ALLEY | COMMERCIAL ALLEY |
|----------------|-------------------|------------------|
| HMA DEPTH | 4" | 4" |
| CSTC | 3" | 4" |
| GRAVEL BALLAST | 6" | 10" |

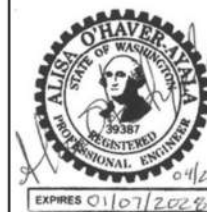


POROUS HMA ALLEY

GENERAL NOTES

- PERMEABLE BALLAST SHALL MEET THE REQUIREMENTS OF WSDOT STD SPECIFICATION 9-03.9(2) WITH THE FOLLOWING EXCEPTIONS: PERCENT FRACTURE SHALL BE 95% AND SHALL BE AT LEAST 2 FRACTURED FACES AND WILL APPLY TO THE COMBINED AGGREGATE ON THE NO. 4 SIEVE IN ACCORDANCE WITH WSDOT FOP FOR AASHTO T 335. LOS ANGELES WEAR, 500 REV SHALL BE 30% MAX. DEGRADATION FACTOR SHALL BE 30 MINIMUM. THE MINIMUM VOID RATIO OF THE AGGREGATE SHALL BE 30% AS DETERMINED BY AASHTO T 19.
- DEPTHS OF POROUS HMA, PERMEABLE BASE, AND PERMEABLE BALLAST SHALL BE PROVIDED BY A GEOTECHNICAL ENGINEER AND STORMWATER ANALYSIS WITH A CONTINUOUS SIMULATION MODEL.
- POROUS HMA ALLEY CAN BE GRADED WITH INVERTED 3% CROSS SLOPE AND VALLEY GUTTER MATCHING "HMA ALLEY WITH VALLEY GUTTER" REQUIREMENTS AS SHOWN ABOVE.
- MINIMUM SURFACE LONGITUDINAL SLOPE SHALL BE 0.5%.
- GEOTEXTILE TO BE PROVIDED WHEN RECOMMENDED BY GEOTECHNICAL PROFESSIONAL AND SHALL BE REQUIRED WHEN FINES IN NATIVE SUBGRADE EXCEED 7% ON THE #200 SIEVE.
- GEOTEXTILE FOR SEPARATION PER WSDOT 9-33.2(1), WOVEN, TABLE 3 AND INSTALLED PER WSDOT 2-12.3(1).
- PERMEABLE PAVEMENT SURFACING SHALL MEET APWA GSP 5-04.3 CONSTRUCTION REQUIREMENTS FOR POROUS ASPHALT (PHMA/PWMA).
- REFER TO R6-02.1 SHEET 2 FOR GENERAL NOTES.

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ALLEY CROSS SECTION



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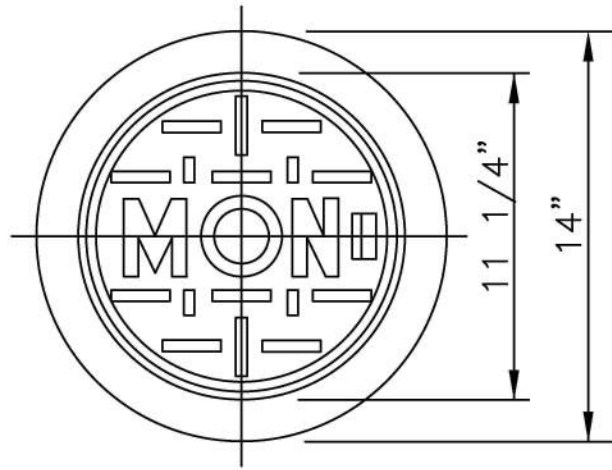
SHEET 1 of 2 N.T.S.

STREET
STANDARD DETAIL
R6-02

ROADWAY AND ALLEY SECTION NOTES

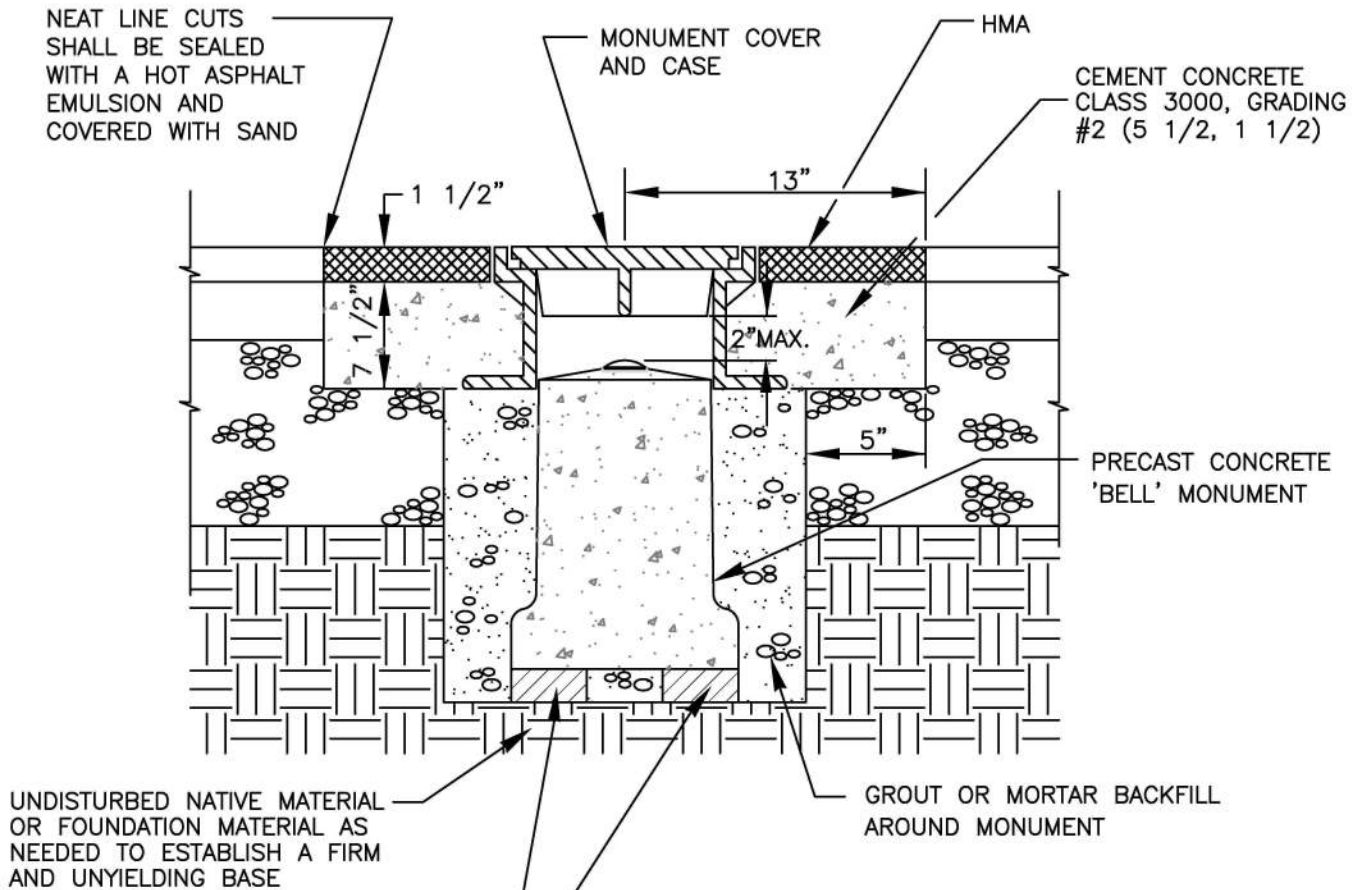
1. ALL DEPTHS ARE MINIMUM COMPACTED DEPTHS. SUBGRADE PREPARATION FOR DENSE HMA AND PCC SHALL MEET THE REQUIREMENTS OF WSDOT STANDARD SPEC. 2-06.3(1). THE UPPER ONE (1) FOOT OF THE SUBGRADE SOILS SHALL BE COMPACTED TO AT LEAST 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MEET THE COMPACTION REQUIREMENTS AND CONTROL ALL WORK. THE CITY OF SUMNER REQUIRES COMPLIANCE TESTS, AT THE CONTRACTORS EXPENSE.
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3. UNLESS A VARIANCE IS GRANTED BY THE CITY ENGINEER OR DESIGNEE, THE CONTRACTOR SHALL PLACE A GEOTEXTILE FABRIC OVER THE EXISTING SUBGRADE. THE SUBGRADE SHALL BE PREPARED AS OUTLINED IN DIVISION 2 OF THE WSDOT SPEC. BEFORE THE PLACING OF THE FABRIC ON THE SUBGRADE. THE CONTRACTOR SHALL PLACE A GEOTEXTILE FABRIC OVER THE PREPARED SUBGRADE WITH A 2 FOOT MINIMUM OVERLAP. THE MATERIAL SHALL BE MIRAFI 500X WOVEN SLIT FILM GEOTEXTILE FABRIC OR AN APPROVED EQUAL (WSDOT SPEC 9-33.2(1), TABLE 3). THE MATERIAL SHALL BE PROPERLY PACKED AND DELIVERED TO THE SITE AS RECOMMENDED BY THE MANUFACTURER. THE CONTRACTOR SHALL NOT BE ALLOWED TO USE ANY PORTION OF A USED ROLL OF FABRIC.
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7. TEMPERATURE SHALL NOT EXCEED 325° F AT DISCHARGE OF THE PLANT NOR LESS THAN 190° F LEAVING THE SPREADER BOX.
8. THE MAXIMUM COMPACTED THICKNESS OF ANY SINGLE LIFT OF ASPHALT PAVEMENT SHALL BY 3" AND MEET WSDOT STANDARD SPEC. 5-04.3(7) TO A MINIMUM COMPACTED DRY DENSITY OF 91% OF THE MAXIMUM DENSITY AS DETERMINED BY WSDOT TEST METHOD SPECIFIED IN STANDARD SPEC. 5-04.3(10).. PERIODIC COMPLIANCE TESTS SHALL BE MADE BY A CERTIFIED TESTING AGENCY AT THE EXPENSE OF THE CONTRACTOR. EACH LIFT SHALL BE COMPACTED IN ACCORDANCE WITH WSDOT SPEC. SECTION 5-04.3(10) TO A MINIMUM AVERAGE COMPACTED DENSITY OF 92% OF THE REFERENCE MAXIMUM DENSITY AS DETERMINED BY WSDOT TEST METHOD 705. PERIODIC COMPLIANCE TESTS SHALL BE MADE AT THE EXPENSE OF THE CONTRACTOR.
9. THE FACE OF THE GUTTER LIP AND EDGES OF EXISTING ASPHALT MEET LINES SHALL BE TACK COATED PRIOR TO PAVEMENT PLACEMENT. WHEN SUCCESSIVE LIFTS OF ASPHALT ARE REQUIRED, A TACK COAT SHALL BE DISTRIBUTED UNIFORMLY OVER THE PREVIOUS LIFT PER WSDOT SECTION 5-04.3(4) AND SHALL BE ALLOWED TO SET TO A TACKY STATE PRIOR TO THE PLACEMENT OF THE NEXT LIFT.
10. ALL MEET LINES BETWEEN LIFTS OF ASPHALT SHALL BE UNIFORM, WITH THE EDGES VERTICAL AND AT THE REQUIRED THICKNESS. IF SUBSEQUENT LIFTS ARE NOT COMPLETED WITHIN 48 HOURS OR THE EDGES HAVE BEEN CONTAMINATED, THE MEET LINES SHALL BE CLEANED AND TACK COATED.
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12. MONUMENTS SHALL NOT BE REMOVED BY THE CONTRACTOR UNTIL PROPERLY REFERENCED BY A LICENSED LAND SURVEYOR. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO HAVE MONUMENTS REFERENCED AND REPLACED BY A LICENSED LAND SURVEYOR PER THE SURVEY RECORDING ACT, SECTION RCW 58.09.130 AND WAC 332-120. NO PROJECT APPROVALS SHALL BE GIVEN UNTIL A COPY OF THE DNR GRANTED PERMIT FOR TEMPORARY MONUMENT REMOVAL OR POTENTIAL DISRUPTION IS RECEIVED BY THE CITY OF SUMNER PUBLIC WORKS DEPARTMENT.
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| | | | | | |
|---|---|-------------------------|------------------------|--------------|-------|
| <p>APPROVED BY CITY ENGINEER</p>  <p>EXPIRES 01/07/2028</p> |  <p>ALLEY CROSS SECTION - NOTES</p> | | | | |
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| SHEET 2 of 2 | R6-02 | | | | |

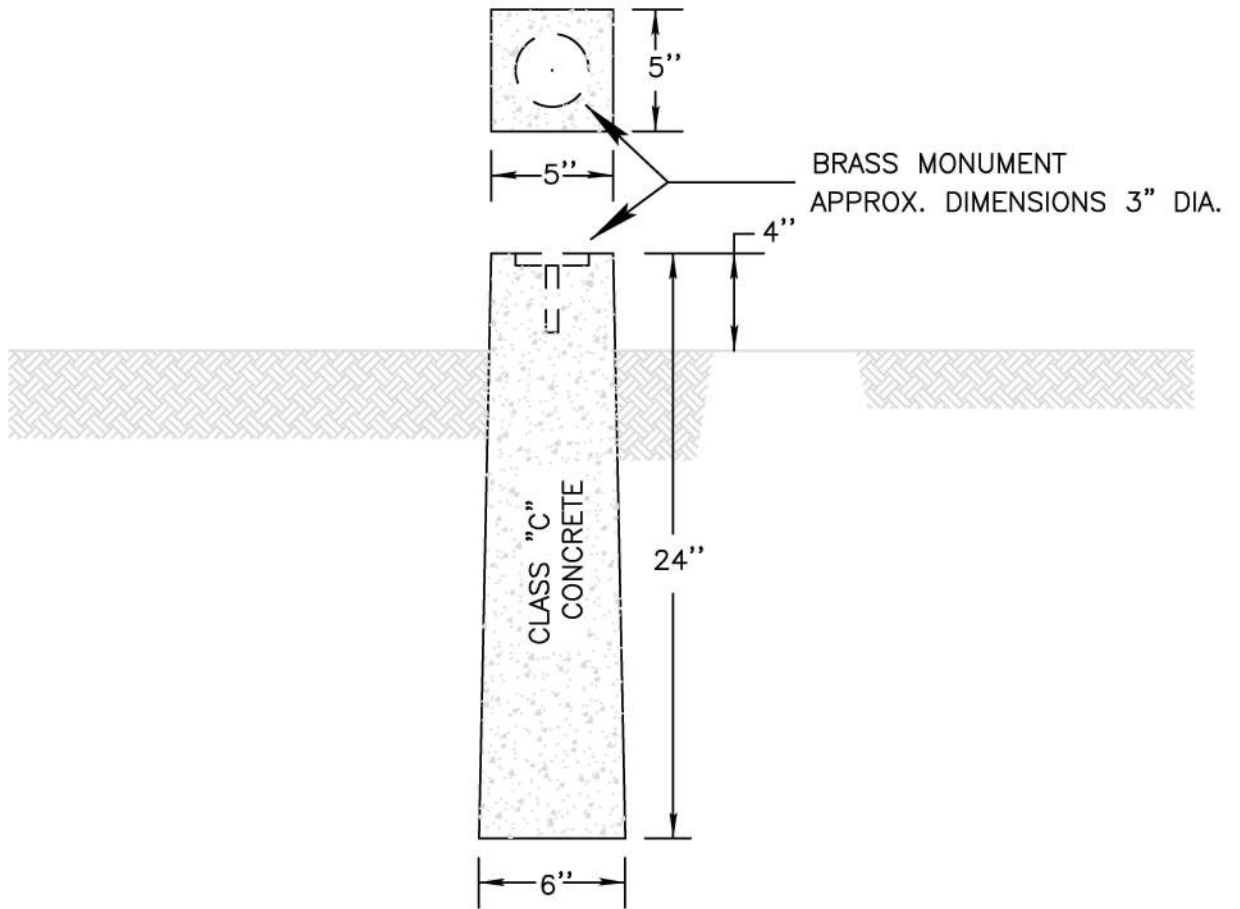


NOTES:

1. MATERIAL SHALL CONFORM TO THE "STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION" PREPARED BY THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION.
2. THE COVER AND SEAT SHALL BE MACHINED SO AS TO HAVE PERFECT CONTACT AROUND THE ENTIRE CIRCUMFERENCE AND FULL WIDTH OF BEARING SURFACE.
3. MONUMENT COVERS SHALL BE REMOVED AND MONUMENTS PROTECTED DURING OVERLAYS. MONUMENT SHALL BE RE-INSTALLED PER STANDARD DETAIL.



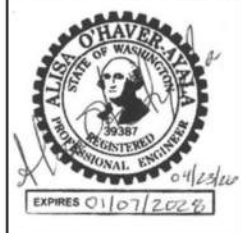
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| APPROVED BY CITY ENGINEER  |  CITY OF SUMNER WASHINGTON | MONUMENT IN ASPHALT AREA |
| TWO (2) MASONRY BRICKS | LAST REVISION: 04/01/26 SHEET 1 of 1 | STREET STANDARD DETAIL R6-03 |



NOTES:

1. THE 24" ENCASEMENTS ARE PRECAST AND USED IN LOCATIONS OTHER THEN ASPHALTIC CONCRETE ROAD PAVEMENTS.
2. CONCRETE MONUMENTS SHALL BE SET AT ALL CORNERS OF THE SUBDIVISION
3. ALL SURVEYS SHALL BE OF SECOND DEGREE ACCURACY
4. LOT CORNERS SHALL BE MARKED WITH A PERMANENT METAL MARKER NOT LESS THAN THREE-EIGHTHS INCH IN DIAMETER AND TWENTY-FOUR INCHES LONG AND DRIVEN FLUSH WITH THE FINISHED GRADE.

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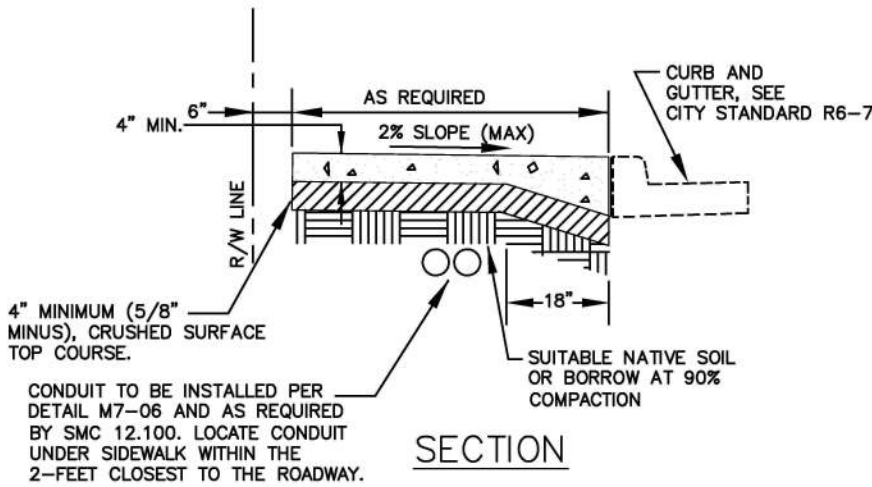
PRECAST MONUMENT
FOR OFF-ROAD USE

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

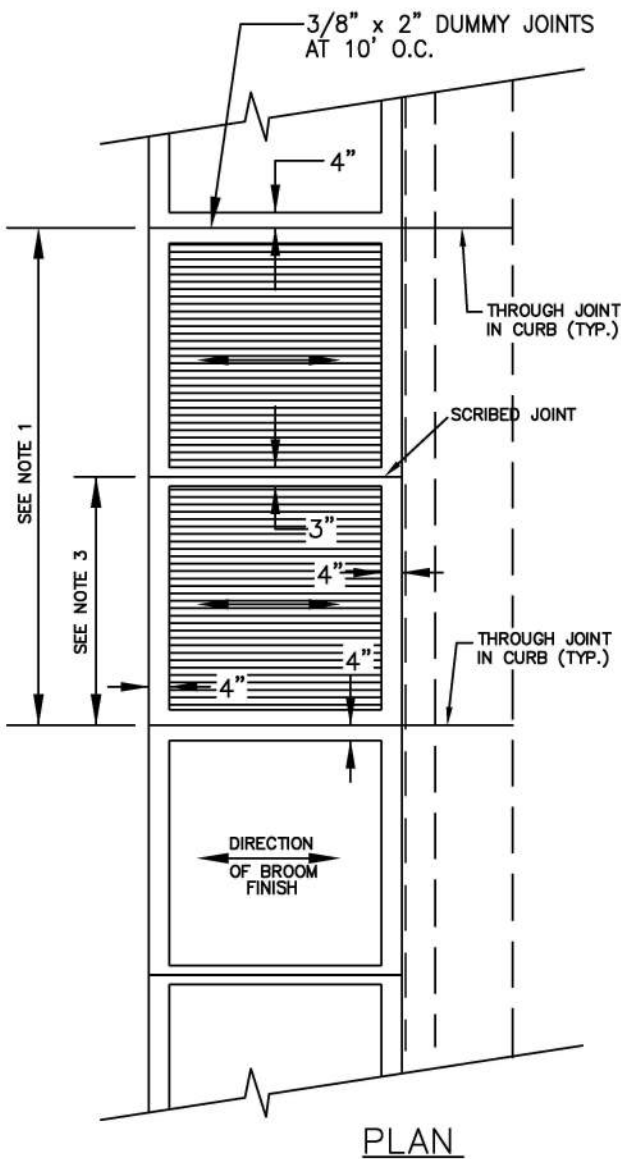
STREET
STANDARD DETAIL
R6-04

MINIMUM SIDEWALK WIDTHS
SEE SUMNER DEVELOPMENT
SPECIFICATIONS, CHAPTER 6.



NOTES:

1. DUMMY JOINTS SHALL BE 3/8" x 2" ASPHALT SATURATED FELT PLACED AT 10' O.C.
2. THRU JOINTS SHALL BE 3/8" ASPHALT SATURATED FELT PLACED AT DRIVEWAY AND ALLEY RETURNS.
3. SCRIBE MARKS SHALL BE 1/2" DEEP AND 1/4" WIDE PLACED AT 5' O.C. FOR 5' SIDEWALKS.
4. ALL JOINTS SHALL BE CLEAN AND EDGED TO A 1/4" RADIUS. JOINTS SHALL BE FLUSH WITH THE FINISHED SURFACE.
5. ALL UTILITY POLES AND STREET SIGN POSTS IN SIDEWALK AREA NOT REQUIRED TO BE RELOCATED BY THE CITY ENGINEER SHALL HAVE A SQUARE SECTION OF CONCRETE SURROUNDED BY 3/8" THRU JOINT MATERIAL (FULL DEPTH) AROUND THE POLE. THE JOINT SHALL BE NO CLOSER THAN 12" TO ANY SIDE OF THE POLE.
6. FORMS SHALL BE INSPECTED PRIOR TO PLACING CONCRETE.
7. CONCRETE SHALL MEET WSDOT SPEC. 6-02.3(2)b.
8. WHERE A SIDEWALK IS TO BE PLACED AGAINST THE CURB AND GUTTER, THE JOINT SHALL BE A COLD JOINT.
9. A 3/8" THRU JOINT IS TO BE PLACED WHENEVER CONCRETE IS POURED AGAINST EXISTING CONCRETE.
10. CONSTRUCTION SHALL BE PER WSDOT 8-14. METHOD OF CURING SHALL BE APPROVED BY CITY ENGINEER.
11. NO CALCIUM IS ALLOWED IN CEMENT CONCRETE MIX FOR STRUCTURES CONSTRUCTED IN RIGHT-OF-WAY.
12. CURB RAMPS SHALL BE CONSTRUCTED TO CURRENT WSDOT STANDARD PLANS, SECTION F.
13. NO PAINTING OR RE-SURFACING OF SIDEWALKS



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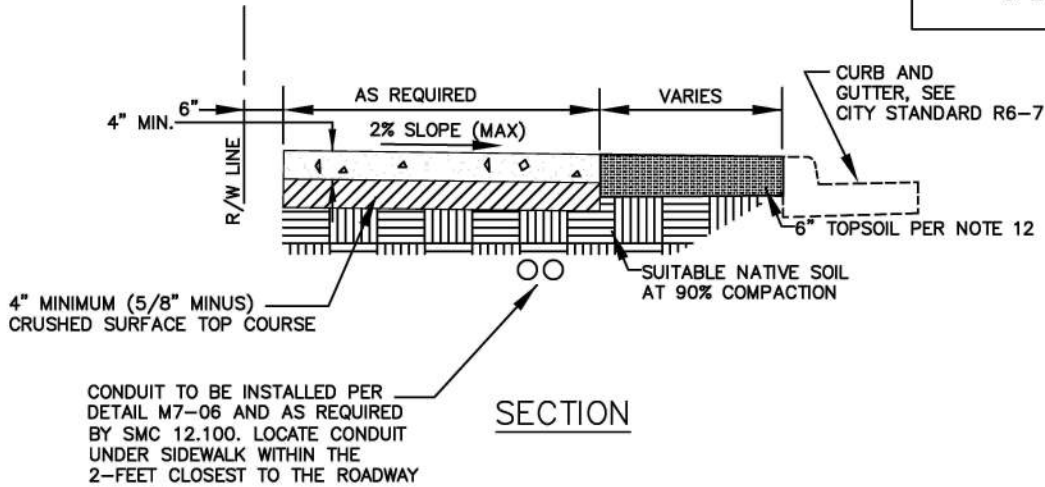


SIDEWALK WITHOUT
PLANTING STRIP

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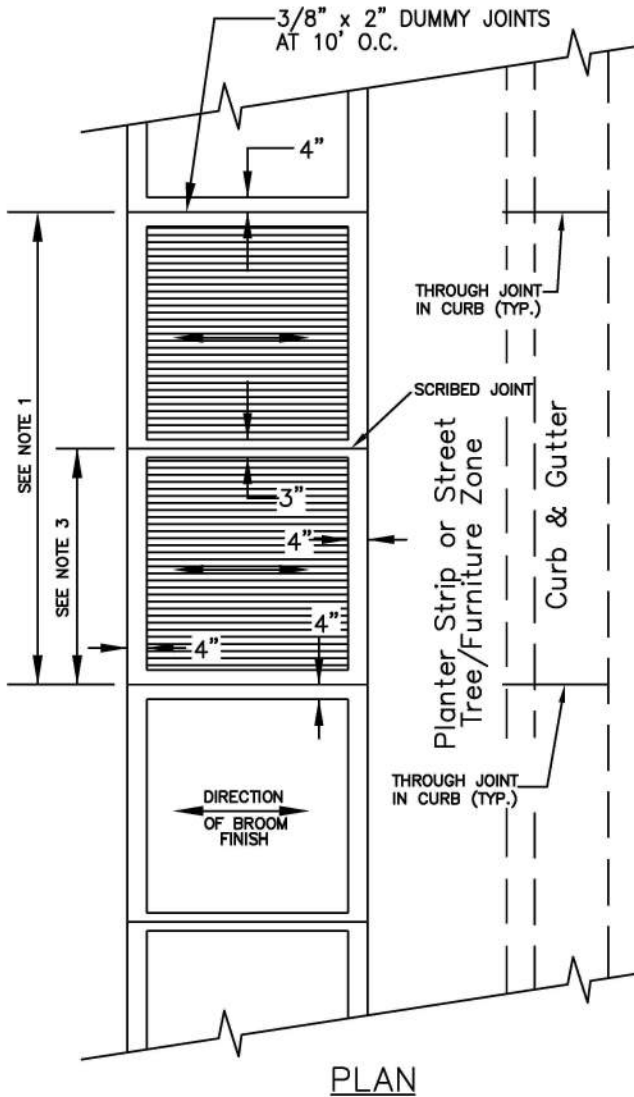
STREET
STANDARD DETAIL
R6-05

SHEET 1 of 1 N.T.S.



NOTES:

1. SEE DETAIL R6-05 FOR ADDITIONAL SIDEWALK NOTES.
2. 6" MINIMUM COMPACTED DEPTH SCREENED WSDOT TYPE 3 TOPSOIL.
3. PLANTER STRIP TOPSOIL TO BE COVERED WITH GRASS SOD, HYDROSEED, OR SEED MAY BE APPLIED IF COVERED WITH PROTECTIVE MULCH TO ASSURE GERMINATION.
4. ALTERNATIVE OPTIONS FOR PLANTER STRIP SURFACING MAY BE PLACED BY THE PROPERTY OWNER. THE ONLY ALLOWABLE OPTIONS ARE 3/4" TO 1-1/4" WASHED ROUND ROCK OR VEGETATION OF ANY TYPE NO MORE THAN 12" TALL. INSTALLATION OF NARROW PERVIOUS PAVER BLOCK CROSSINGS ARE ALLOWED AT NO LESS THAN 20' SPACING.
5. IN THE TOWN CENTER PLAN AREA, THE PLANTER STRIP IS REPLACED WITH THE 6' (MIN.) STREET TREE/FURNITURE ZONE AND THE SIDEWALK VARIES. REFER TO THE TOWN CENTER PLAN FOR WHAT IS ALLOWABLE IN THE PUBLIC REALM.
6. ON STREETS WITHIN THE TOWN CENTER PLAN, NO PLANTERS WILL BE ALLOWED EXCEPT IN A TRANSITION ZONE OR WHERE APPROVED BY THE CITY ENGINEER, OR DESIGNEE.
7. REFER TO DETAIL R6-13 (2 OF 2) FOR TREE GRATES.



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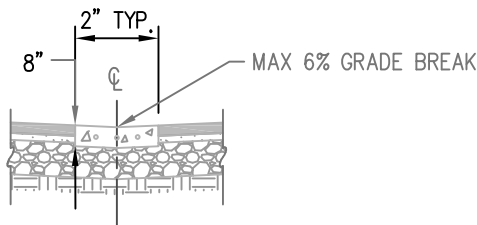
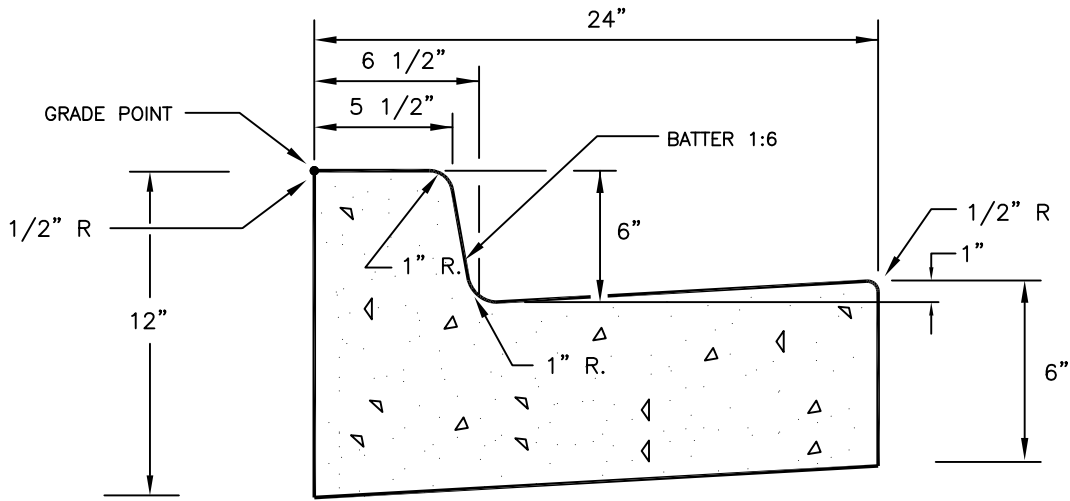


SIDEWALK WITH PLANTER STRIP
OR STREET TREE/FURNITURE ZONE

LAST REVISION: 04/01/26

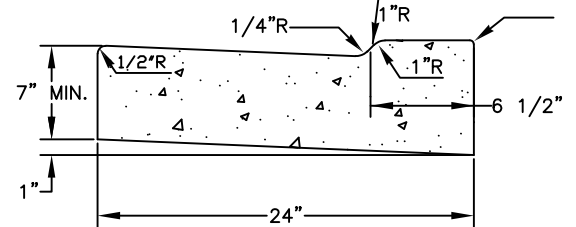
STREET
STANDARD DETAIL
R6-06

SHEET 1 of 1 N.T.S.



VALLEY GUTTER DETAIL

NOTE: TRANSITION BETWEEN CURB AND GUTTER AT CURB RAMPS SHALL BE "FLUSH" AND 1/2" LIP FOR STANDARD DRIVEWAY APPROACH.



**TYPICAL SECTION
DEPRESSED CURB AND GUTTER**

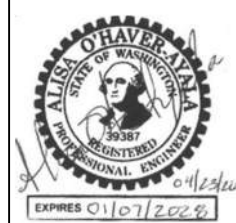
NOTES:

1. 2' WIDE X 8" THICK CONCRETE "V" GUTTER WITH (3) #4 REBAR SHALL BE INSTALLED UNLESS SEAMLESS HMA ALLEY CAN BE CONSTRUCTED.
2. SEE STANDARD DETAIL R6-2.2 FOR ADDITIONAL NOTES.

NOTES:

1. CONTRACTION JOINTS SHALL BE 1/2" x 2 1/4" ASPHALT SATURATED EXPANSION JOINT PLACED IN ALL EXPOSED SURFACES OF CURB AND GUTTER AND SPACED AT 10' O.C.
2. THRU JOINTS SHALL BE 1/2" ASPHALT SATURATED EXPANSION JOINT PLACED AT POINTS OF TANGENCY ON CURVES, AT CATCH BASINS, AND AT EDGES OF ALLEY AND DRIVEWAYS. THE MAXIMUM DISTANCE BETWEEN THRU JOINTS SHALL BE 100'. ALL JOINTS SHALL BE CLEAN AND IN GUTTER SECTIONS THEY SHALL BE EDGED.
3. CONCRETE SHALL BE COMMERCIAL CONCRETE MIX PER WSDOT 6-02.3(2)B.
4. FORMS SHALL BE STEEL AND SHALL BE SET TRUE TO LINE AND GRADE AND SECURELY STAKED PRIOR TO CONCRETE PLACEMENT. FULL DEPTH DIVISION PLATES ARE ONLY TO BE USED WHERE THRU JOINTS ARE TO BE PLACED.
5. THE 1" RADIUS ON THE UPPER FACE OF THE CURB MAY BE FORMED BY AN EDGER TOOL OR BUILT INTO THE FACE FORM. THE 1" RADIUS AT THE BOTTOM FACE OF THE CURB SHALL BE FORMED BY THE FACE FORM.
6. ALL CONSTRUCTION SHALL CONFORM TO THESE SPECIFICATIONS AND TO THE WSDOT STANDARD SPECIFICATIONS SECTION 8-04. METHOD OF CURING TO BE APPROVED BY CITY ENGINEER.
7. NO CALCIUM IS ALLOWED IN CEMENT CONCRETE MIX FOR STRUCTURES CONSTRUCTED IN THE RIGHT-OF-WAY.
8. SEE DETAIL R6-01 FOR REQUIRED CSTC, CSBC AND BALLAST CURB.

APPROVED BY CITY ENGINEER

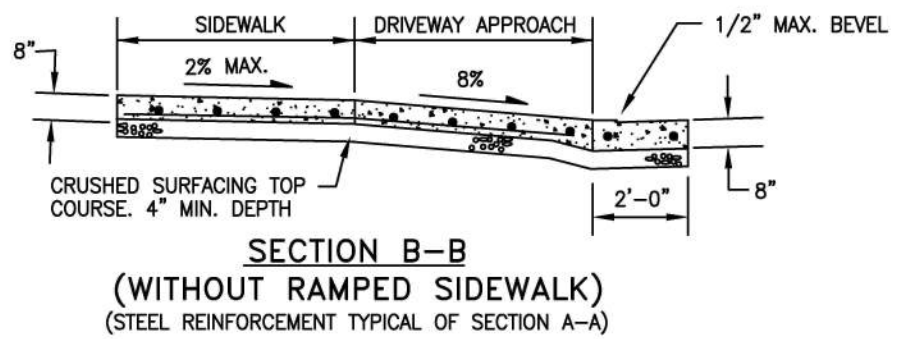
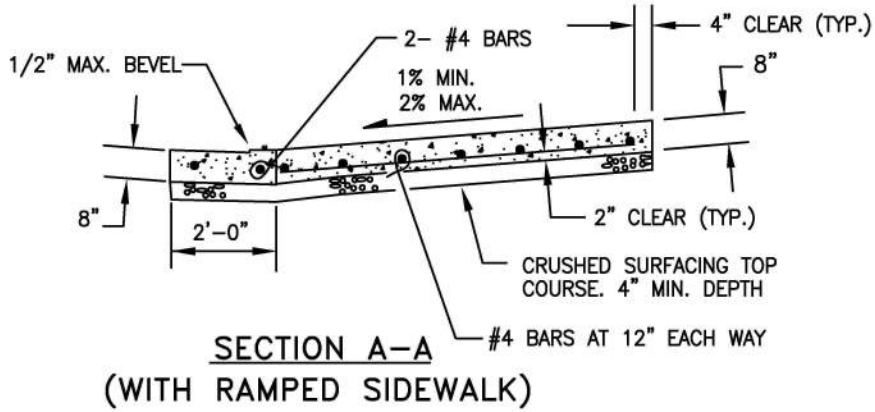
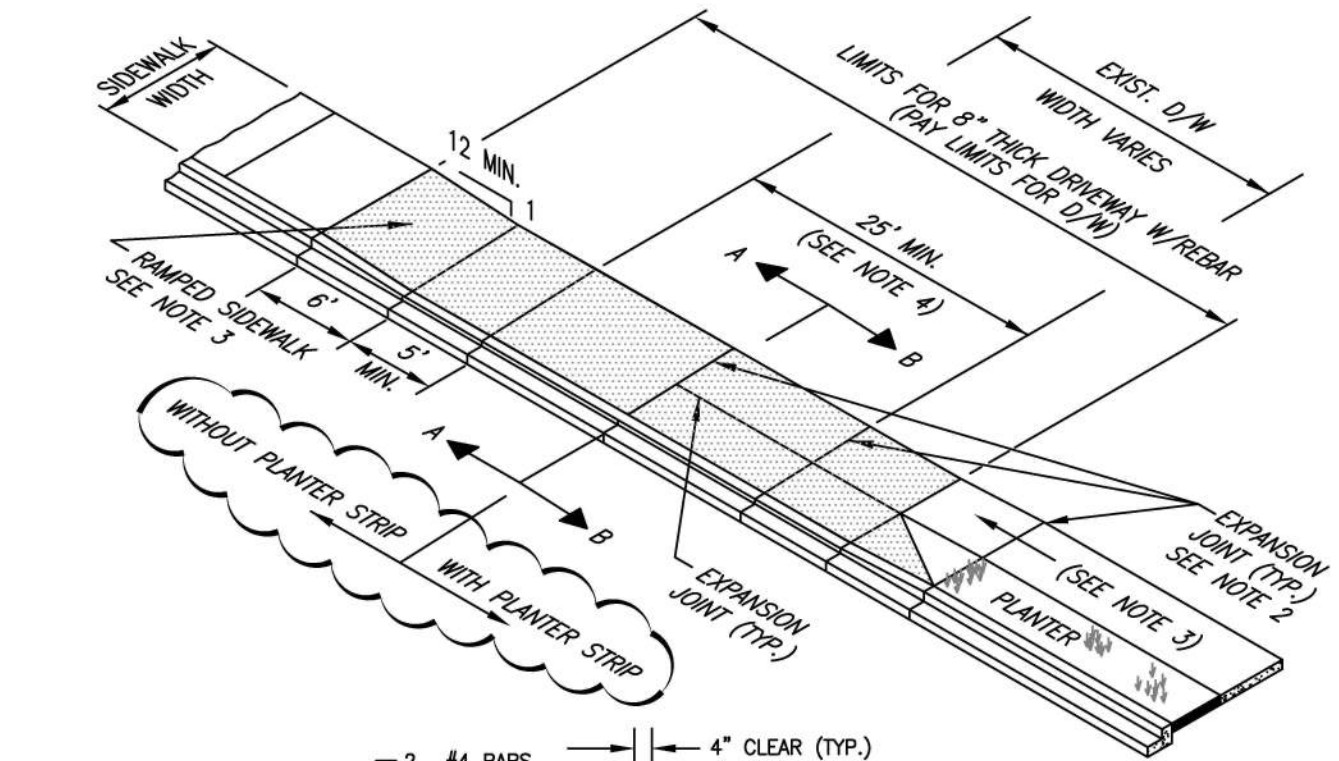


CURB AND GUTTER

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

STREET
STANDARD DETAIL
R6-07



NOTES:

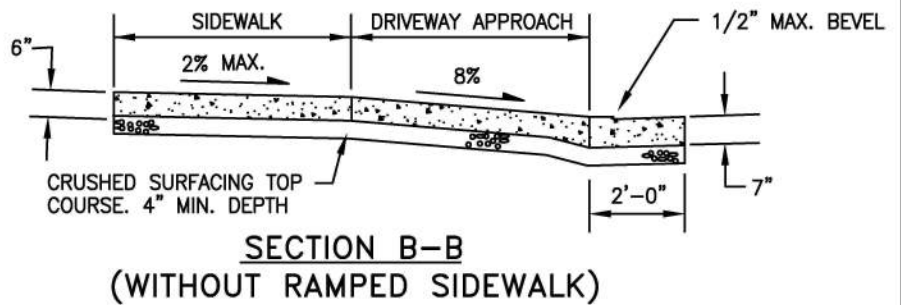
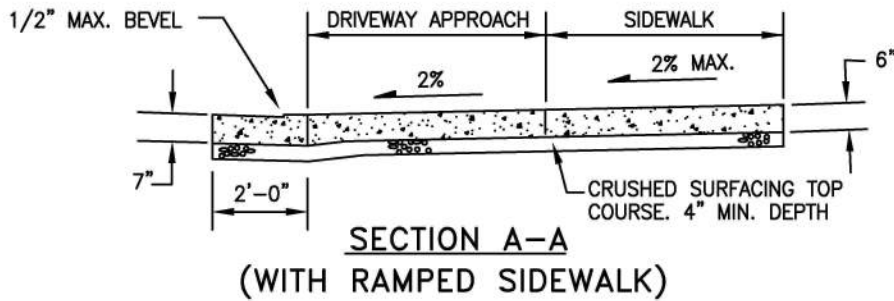
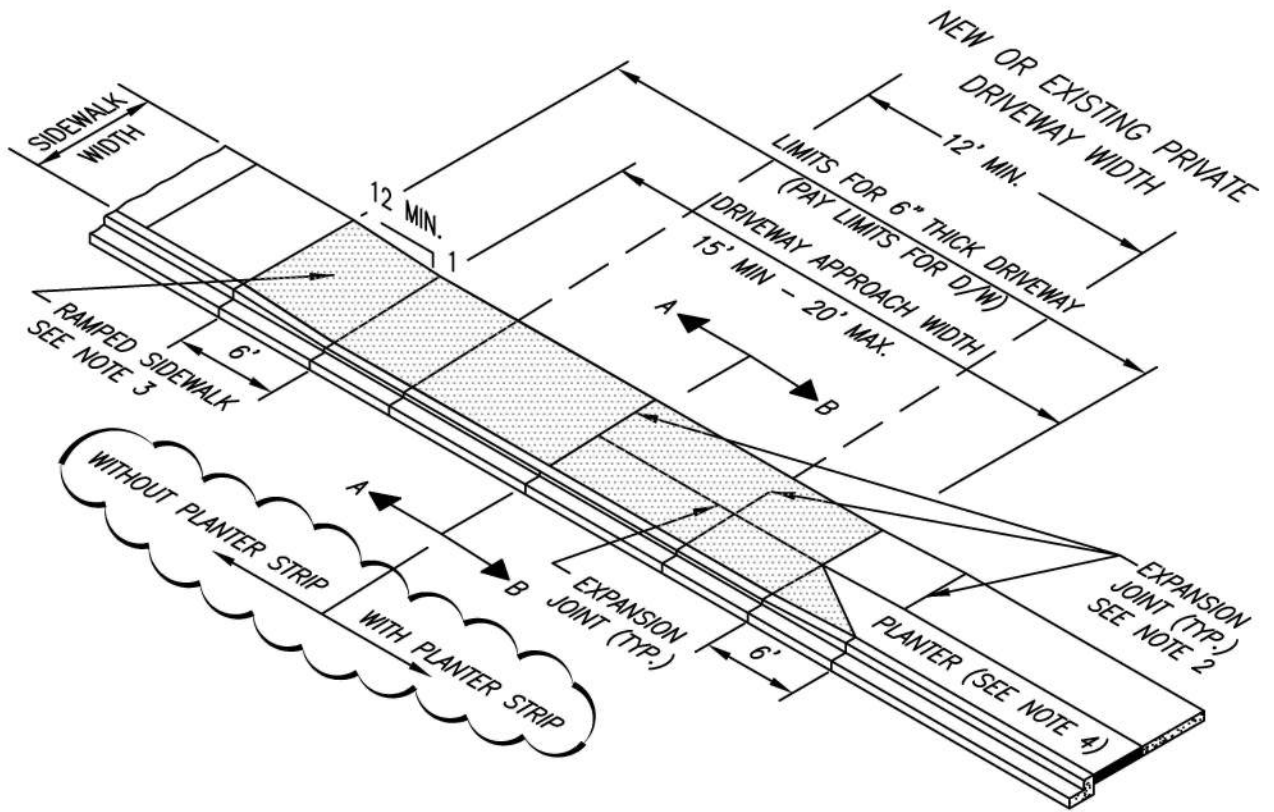
1. DETAILS SHOWN ARE TYPICAL, THE ENGINEER RESERVES THE RIGHT TO REVISE DETAILS TO BETTER MATCH FIELD CONDITIONS.
2. EXPANSION JOINTS SHALL BE PLACED AT 10' MAXIMUM SPACING. ELASTOMERIC JOINT MATERIAL SHALL BE IN CONFORMANCE TO SECTION 9-04.1 (4) OF THE WSDOT STANDARD SPECIFICATIONS.
3. RAMPING OF THE SIDEWALK IS ONLY ALLOWED WHEN NECESSARY TO MAINTAIN 8% OR LESS IN THE DRIVEWAY APPROACH. MAINTAINING RELATIVELY THE SAME ELEVATION AS THE SIDEWALK IS PREFERRED AT THE BACK OF THE DRIVEWAY.
4. PAY LIMITS OF DRIVEWAY =
5. HEAVY COMMERCIAL DRIVEWAY APPROACHES SHALL BE DESIGNED FOR APPROPRIATE TURNING MOVEMENTS AND TRAFFIC LOADS AND SUBMITTED TO THE CITY FOR APPROVAL BY THE CITY ENGINEER.

APPROVED BY CITY ENGINEER

CITY OF **SUMNER**
 WASHINGTON


COMMERCIAL CEMENT CONCRETE DRIVEWAY APPROACH

| | |
|-------------------------|------------------------|
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL |
| SHEET 1 of 1 | N.T.S. R6-08 |




NOTES:

1. DETAILS SHOWN ARE TYPICAL, THE ENGINEER RESERVES THE RIGHT TO REVISE DETAILS TO BETTER MATCH FIELD CONDITIONS.
2. EXPANSION JOINTS SHALL BE PLACED AT 10' MAXIMUM SPACING. ELASTOMERIC JOINT MATERIAL SHALL BE IN CONFORMANCE TO SECTION 9-04.1 (4) OF THE WSDOT STANDARD SPECIFICATIONS.
3. RAMPING OF THE SIDEWALK IS ONLY ALLOWED WHEN NECESSARY TO MAINTAIN 8% OR LESS IN THE DRIVEWAY APPROACH. MAINTAINING RELATIVELY THE SAME ELEVATION AS THE SIDEWALK IS PREFERRED AT THE BACK OF THE DRIVEWAY.
4. PLANTER OPTION TO BE APPROVED BY THE CITY OF SUMNER. ALL DRIVEWAYS ARE TO BE SYMMETRICAL.

5. PAY LIMITS OF DRIVEWAY = 

APPROVED BY CITY ENGINEER



EXPIRES 01/07/2028

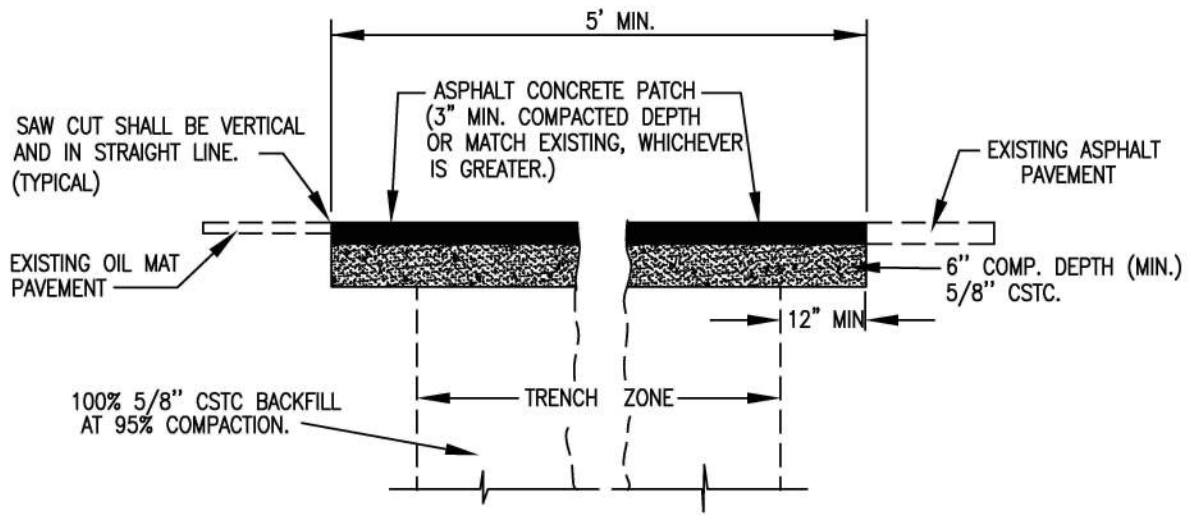
CITY OF SUMNER WASHINGTON

RESIDENTIAL CONCRETE DRIVEWAY APPROACH

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

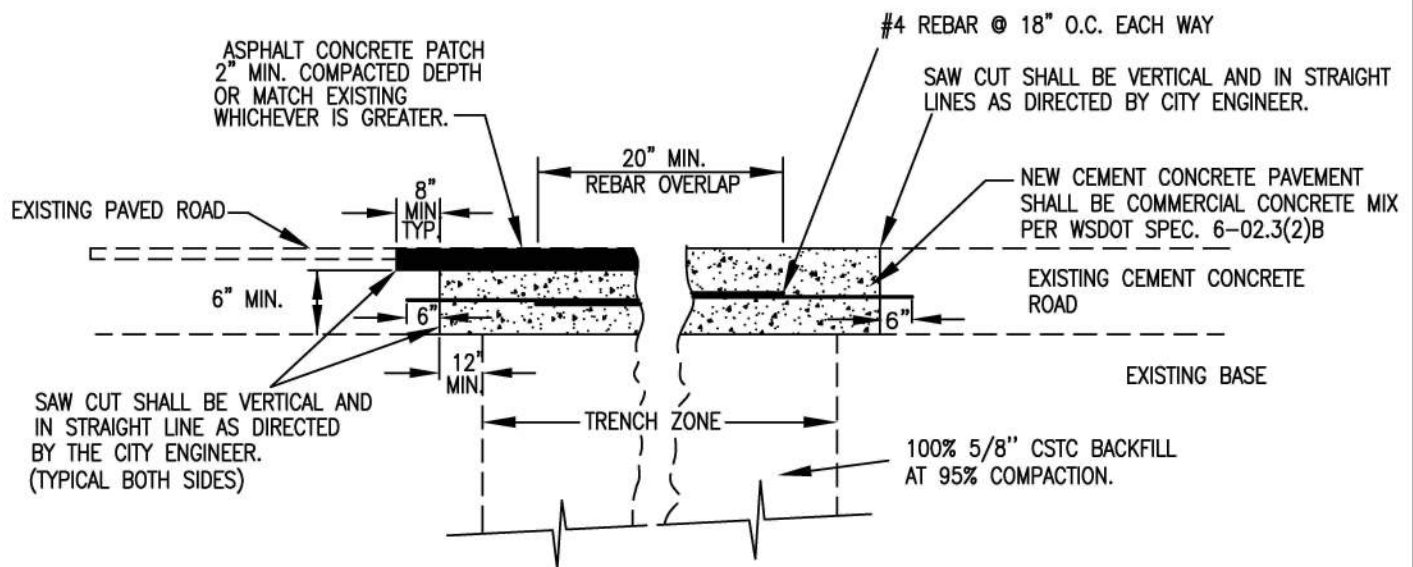
STREET STANDARD DETAIL R6-09



NOTE:

APPLY TACK COAT AND SEALING TO ALL VERTICAL JOINTS. ALL MANHOLE FRAMES, CATCH BASINS FRAMES, VALVE BOXES, ETC., WILL ALSO BE TACK COATED PRIOR TO PATCHING. TACK COAT SHALL BE CSS-1.

TYPICAL PATCH FOR FLEXIBLE PAVEMENT





NOTES:

1. CONCRETE PATCH SHALL BE COVERED WITH STEEL PLATE AND ALLOWED TO CURE FOR SEVEN DAYS PRIOR TO PLACEMENT OF ASPHALT PAVING OR OPENING FOR TRAFFIC.
2. CONCRETE JOINTS IN CONCRETE SHALL MATCH LOCATIONS OF CONCRETE JOINTS IN ADJACENT PANELS.
3. MUST PATCH BACK TO PREVIOUS ASPHALT SECTION..

TYPICAL PATCH FOR RIGID PAVEMENT

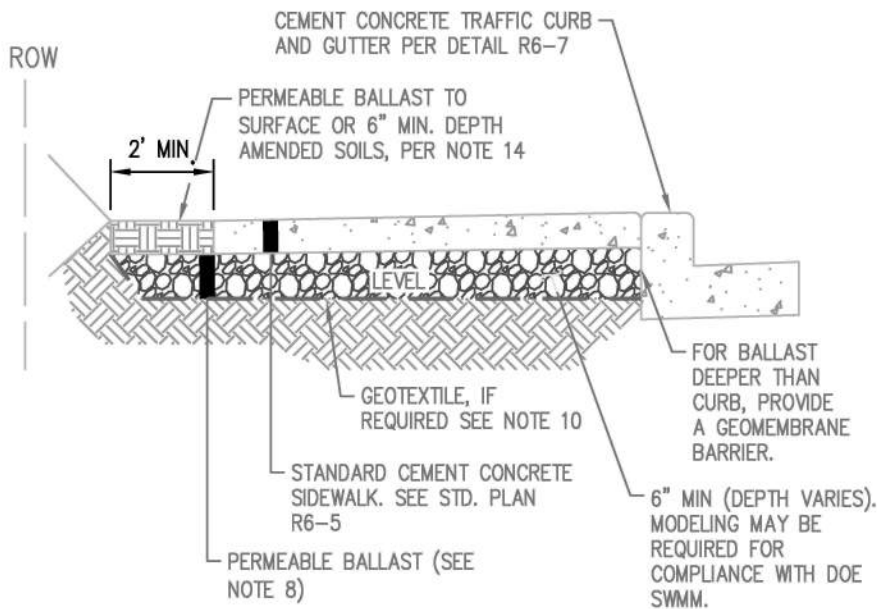
GENERAL NOTES:

1. REFER TO SMC 12.100 DIG ONCE POLICY REGARDING PAVEMENT PATCHING LIMIT REQUIREMENTS.
2. NO PATCHING ALLOWED FOR SIDEWALKS, ONLY FULL PANEL REPLACEMENT.

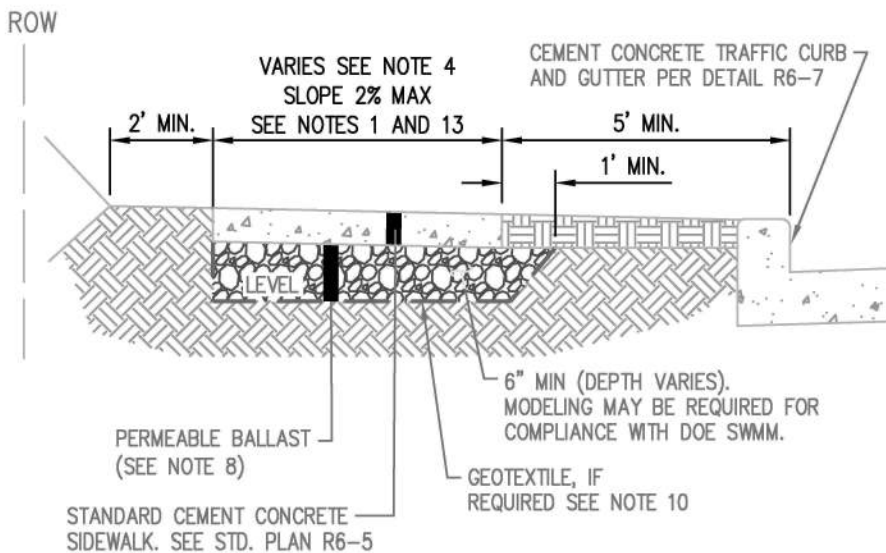
| | |
|---|--|
| APPROVED BY CITY ENGINEER  |  CITY OF SUMNER WASHINGTON |
| PAVEMENT PATCHING | |
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL R6-10 |
| SHEET 1 of 1 | N.T.S. |

NOTES

1. SIDEWALK SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH ADA STANDARDS FOR ACCESSIBLE DESIGN, 28 CFR, PART 35 AND AS SUPPLEMENTED BY THE PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES (PROWAG).
2. WHEN PLACING WALK ADJACENT TO EXISTING CURB AND GUTTER, CURB AND GUTTER WILL BE REPAIRED AS NECESSARY BEFORE PLACING CONCRETE FORMS.
4. COMBINATION WALK SHALL BE A 7" MIN. ON ALL COMMERCIAL SITES AND ARTERIALS STREETS. COMBINATION WALK SHALL BE A MINIMUM OF 5' ON NON-ARTERIAL STREETS. DIMENSIONS ARE FROM BACK OF CURB TO BACK OF WALK. SEE CONTRACT PLANS FOR WIDTH AND PLACEMENT OF SIDEWALK.
5. ALL EXPANSION JOINTS SHALL BE FULL DEPTH WITH 3/8" PREMOLDED JOINT FILLER.
6. ALL JOINTS SHALL BE CLEANED AND EDGED. EXTERNAL EDGES SHALL BE 1/2" RADIUS. INTERNAL JOINTS SHALL BE 1/4" RADIUS.
7. SUBGRADE PREPARATION SHALL MEET APWA GSP 2-06.3(3) SUBGRADE FOR PERMEABLE PAVEMENTS.
8. PERMEABLE BALLAST SHALL MEET APWA GSP 4-04.2 GRAVEL BASE AND 9-03.9(2). OPT1 PAVEMENT BALLAST.
9. ALL SOFT AND YIELDING FOUNDATION MATERIAL SHALL BE REMOVED AND REPLACED WITH BALLAST PER APWA GSP 4-04.2 GRAVEL BASE AND 9-03.9(2). OPT1 PERMEABLE BALLAST.
10. GEOTEXTILE FABRIC MAY BE REQUIRED BETWEEN NATIVE SOILS OR AMENDED SOILS AND PERMEABLE BALLAST PER THE RECOMMENDATION OF THE GEOTECHNICAL PROFESSIONAL. GEOTEXTILE SHALL BE PER WSDOT 9.33.2(1), TABLES 1 AND 2, NON-WOVEN, MODERATE SURVIVABILITY.
11. WHERE BALLASTED SIDEWALK IS INSTALLED ADJACENT TO PERMEABLE ROADWAY, THE PERMEABLE BALLAST MAY EXTEND FROM THE SIDEWALK TO THE ROADWAY SECTION.
12. WHERE NEEDED, ADJUST BALLAST IN PLANTING STRIP TO ACCOMMODATE PLANTS. KEEP PERMEABLE BALLAST A MINIMUM 2 FEET FROM TRUNK OF TREES.
13. PLANTING STRIP SOILS SHALL BE PER BMP L613, IF APPLICABLE; OR SCARIFY OR TILL SUBGRADE TO 3 INCH DEPTH. PLACE 3-INCHES OF TOPSOIL ON SURFACE AND TILL INTO 5-INCHES OF SITE SOIL. INSTALL 3-INCHES OF ARBORIST WOOD CHIP MULCH OR AS SPECIFIED ON PLANS. TOPSOIL LAYER WITH A MINIMUM ORGANIC MATTER CONTENT OF 10% DRY WEIGHT IN PLANTING BEDS, AND 5% IN TURF AREAS, AND A PH FROM 6.0 TO 8.0 OR MATCHING THE PH OF THE ORIGINAL UNDISTURBED SOIL.

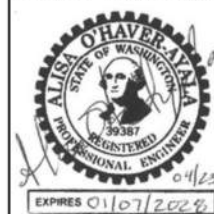


COMBINATION SIDEWALK SECTION
NTS



SIDEWALK WITH PLANTER STRIP
NTS

APPROVED BY CITY ENGINEER



BALLASTED SIDEWALK

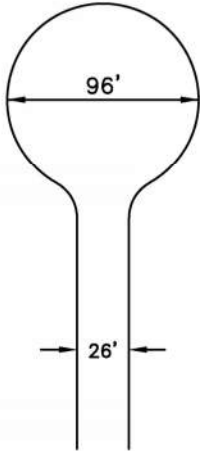
LAST REVISION: 04/01/26

STREET
STANDARD DETAIL

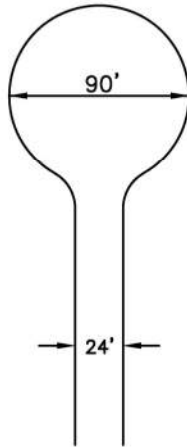
SHEET 1 of 1 N.T.S.

R6-11

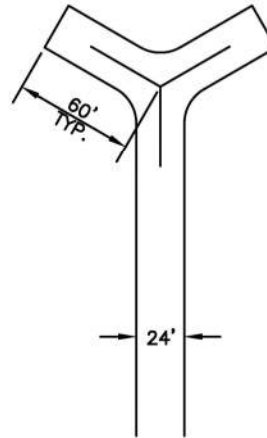
96' DIAMETER
CUL-DE-SAC



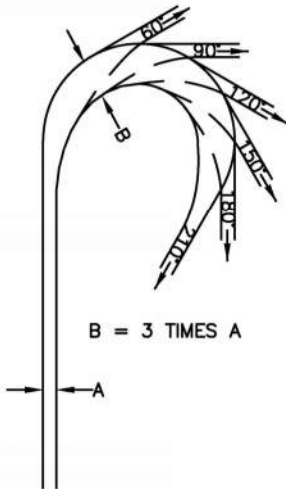
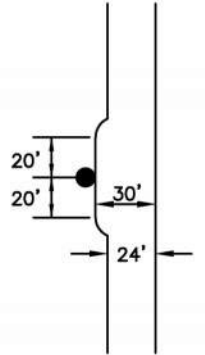
90' DIAMETER
CUL-DE-SAC



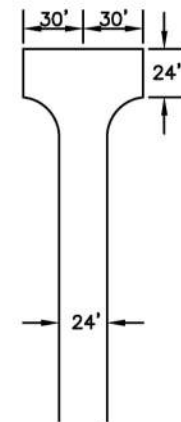
ACCEPTABLE
ALTERNATIVE TO
120' HAMMERHEAD



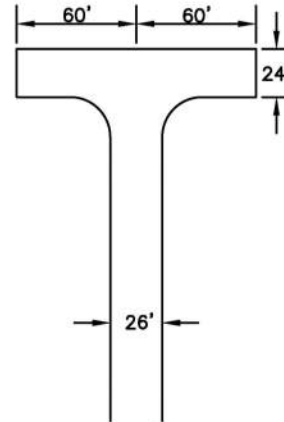
MINIMUM CLEARANCE
AROUND A FIRE
HYDRANT



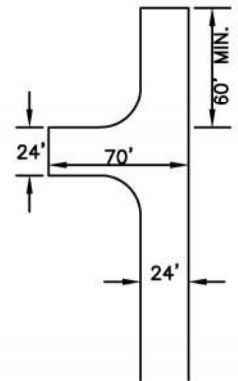
SEMITRAILER WHEELTRACKS
48' RADIUS



60'
HAMMERHEAD

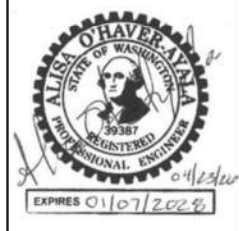


120'
HAMMERHEAD



ACCEPTABLE
ALTERNATIVE TO
120' HAMMERHEAD

APPROVED BY CITY ENGINEER

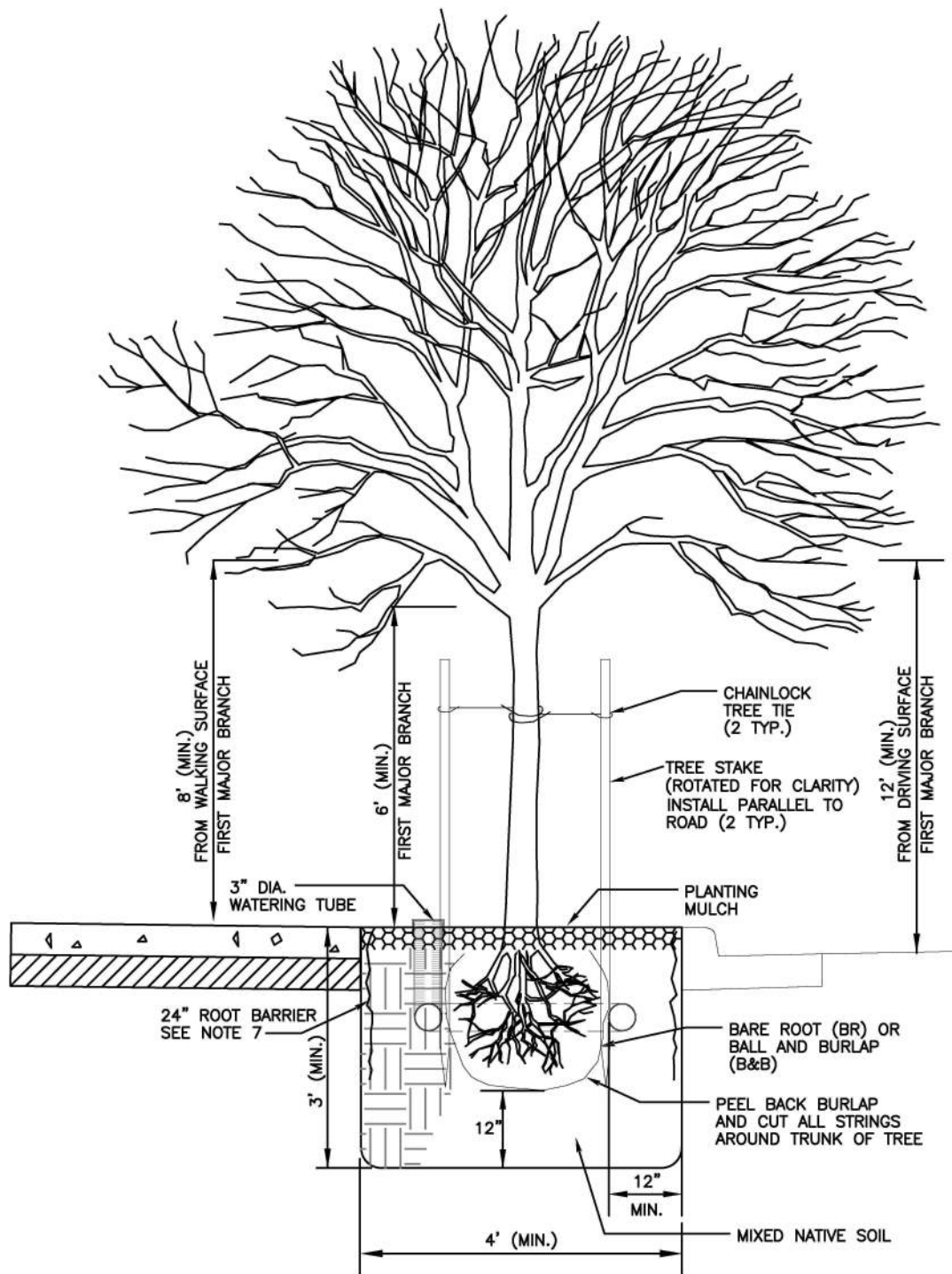


PRIVATE ACCESS
ROADWAYS

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

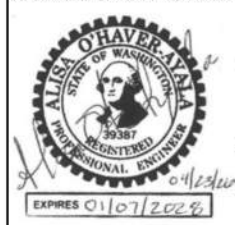
STREET
STANDARD DETAIL
R6-12



NOTES:

- DO NOT DAMAGE ROOT BALLS WHEN PLANTING. REMOVE ALL WIRE, STRING AND BURLAP FROM TOP AND SIDES OF BALL ONLY AFTER PLACING IN HOLE. CUT AND SPREAD ROOTS TO ELIMINATE ROOT CIRCLING.
- SET TREE STRAIGHT AND ROOTBALL ON SOLID GROUND. TOP OF ROOTBALL MUST BE AT OR SLIGHTLY ABOVE FINISHED GRADE.
- PLACE OSMOCOTE PLUS 15-9-12 SLOW RELEASE FERTILIZER OR SIMILAR AT MANUFACTURERS RECOMMENDED RATE EVENLY OVER THE SOIL OF THE PLANTING PIT.
- COVER A MINIMUM OF 2' RADIUS FROM TREE TRUNK WITH 3" DEPTH OF COMPOSTED MEDIUM/COURSE BARK MULCH. PULL MULCH 3" AWAY FROM TRUNK.
- STAKE TREES OUTSIDE OF ROOTBALL AND PARALLEL TO STREET. USE 2" X6' TREATED LODGEPOLE PINE TREE STAKES. USE 1" HEAVY CHAINLOCK TREE TIES OR SIMILAR. REMOVE AFTER ONE YEAR.
- INSTALL 3" DIA. POLYETHYLENE CORRUGATED DRAIN PIPE FOR A WATERING TUBE. WATERING TUBE TO BE INSTALLED AT OR JUST BELOW MIDPOINT OF THE ROOT BALL AND EXTEND TO ABOVE FINISHED GRADE AND CAPPED.
- PLACE ROOT BARRIER FLUSH AGAINST CURB AND SIDEWALK A MINIMUM OF 6' IN BOTH DIRECTIONS OF TREE CENTERLINE.
- BACKFILL AROUND ROOTBALL WITH TOPSOIL TYPE C PER WSDOT STD PLANS 9-14.1(3)
- SEE DEVELOPMENT SERVICES DEPARTMENT FOR APPROVED STREET TREE LIST.
- SEE STANDARD DETAIL R6-13.2 FOR TREE GRATE AND FRAME DETAILS.

APPROVED BY CITY ENGINEER

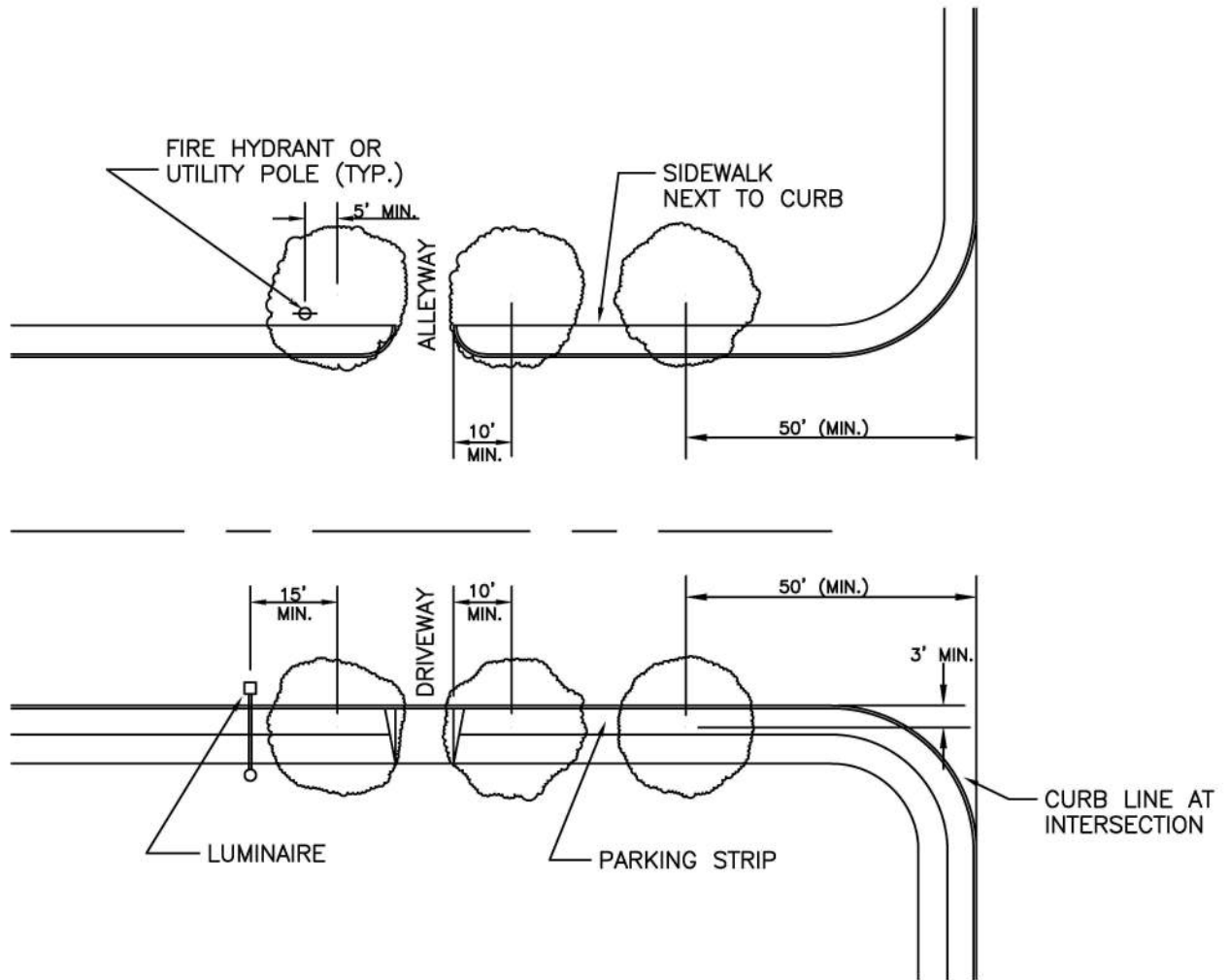


STREET TREES

LAST REVISION: 04/01/26



SHEET 1 of 1 N.T.S.

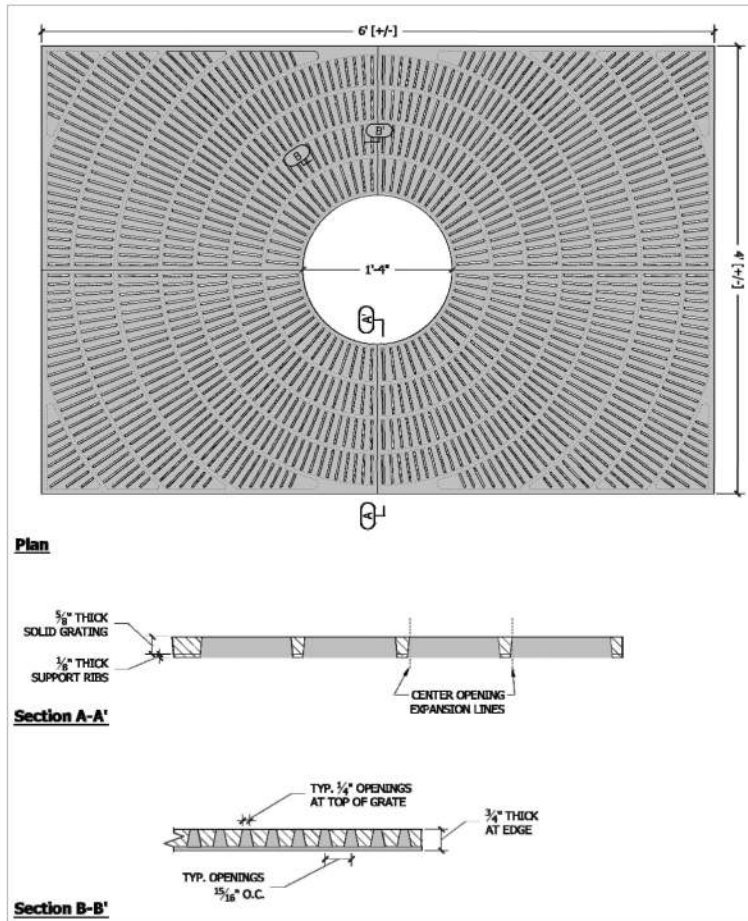
STREET
STANDARD DETAIL
R6-13.1



NOTES:

1. MINIMUM DISTANCE FROM CENTER OF ANY TREE TO THE NEAREST EDGE OF CURB SHALL BE 3 FEET.
2. TREES SHALL BE STAKED IN A MANNER NOT TO OBSTRUCT SIDEWALK TRAFFIC.
3. IN CASE OF BLOCK-OUTS, MIN CLEAR SIDEWALK WIDTH SHALL BE 5 FEET IN RESIDENTIAL AND 8 FEET IN BUSINESS DISTRICTS.
4. SEE STANDARD DETAIL R6-14 FOR THE LIST OF APPROVED TREES.
5. 6' MINIMUM HORIZONTAL DISTANCE TO ANY WET UTILITY (SEWER, STORM, WATER) INCLUDING MAINS AND LATERALS.
6. TREE PLACEMENT SHALL NOT BLOCK ROADWAY SIGNAGE.

| | | |
|---|---|--|
| APPROVED BY CITY ENGINEER  |  | |
| | STREET TREE PLACEMENT | |
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL R6-13.2 | |
| SHEET 1 of 1 | N.T.S. | |



TREE GRATE FRAME, 4' X 6'
TYPE "S" PEDESTRIAN DUTY

SPECIFICATIONS

- MATERIAL WILL BE HIGH QUALITY 100% RECYCLED GREY IRON; ASTM A48 CLASS 35B OR BETTER; HARDNESS 170-223 BRINNELL.

- FINISH WILL BE NATURAL PATINA OF RAW IRON.

- DIMENSIONS ARE NOMINAL

NOTES:

- 1) CAST IN FOUR PIECES.
- 2) GRATE IS 3/4" THICK AT EDGE.
- 3) CENTER OPENING EXPANSIONS AT 2'-0" AND 2'-7"
- 4) NO OPENINGS GREATER THAN 1/4", IN CONFORMANCE WITH ADA ACCESSIBILITY GUIDELINES.

- FRAMES ARE CONSTRUCTED OF MILD STEEL ASTM A36.

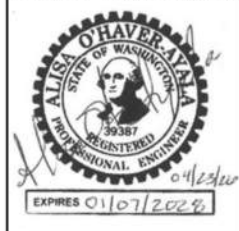
- FINISH WILL BE NATURAL PATINA OF RAW STEEL.

- FRAME IS LOAD RATED FOR PEDESTRIAN TRAFFIC ONLY.

- TYPICAL 1/8" HORIZONTAL GAP BETWEEN GRATE AND FRAME, ALL VISIBLE WELDS TO BE GROUND SMOOTH ON OUTSIDE EDGES. FRAMES WILL BE TRUE TO SQUARE OR DIAMETER. TOP OF GRATE FLUSH WITH GRADE OF SURROUNDING TOPPING MATERIAL (PAVER, CONCRETE SLAB, ETC.).

- DIMENSIONS ARE NOMINAL

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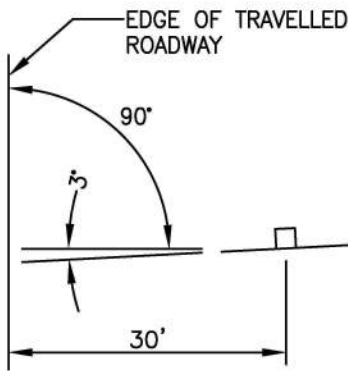


TREE GRATE

LAST REVISION: 04/01/26

STREET
STANDARD DETAIL
R6-14

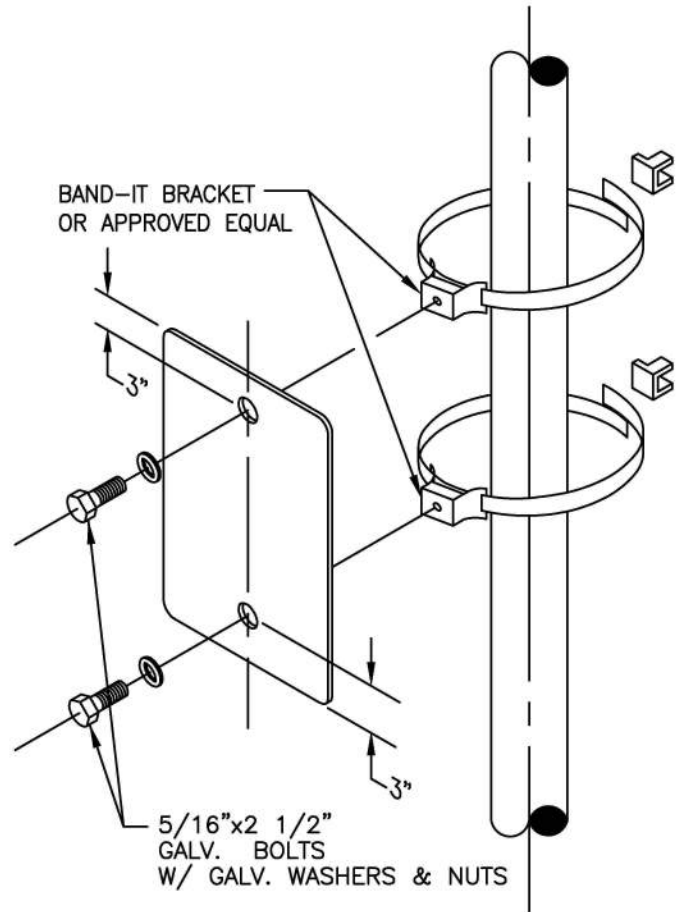
SHEET 1 of 1 N.T.S.



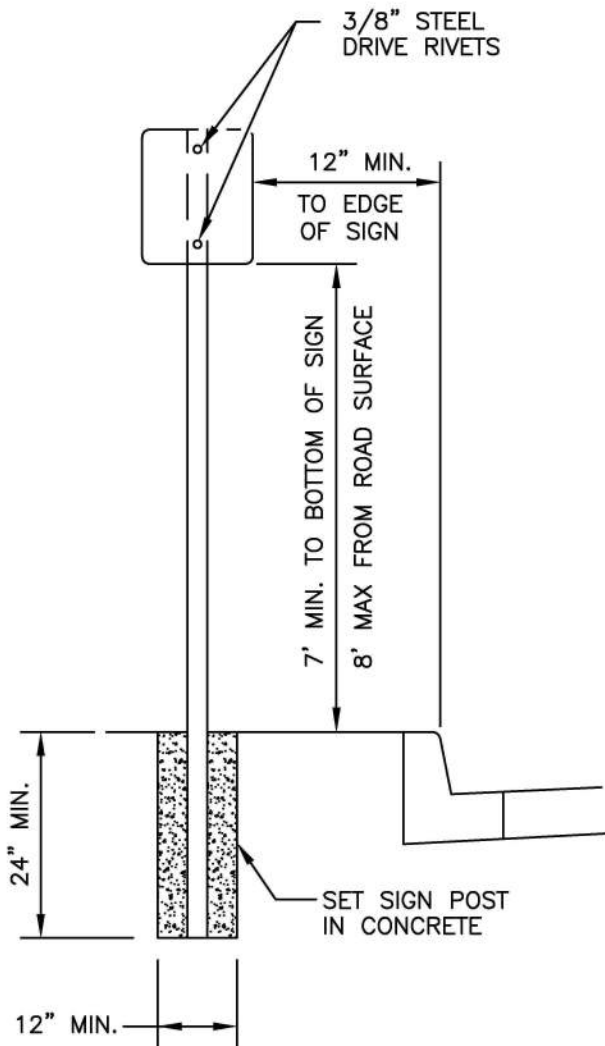
FOR REFLECTORIZED SIGNS:
MOUNT FACING 3' OUTWARD
IF SIGN IS WITHIN 30 FEET
OF ROADWAY

"NO PARKING" SIGNS SHALL BE
INSTALLED AT 45° TO ROADWAY

SIGN FACE ORIENTATION



**MOUNTING ON STREET LIGHT
STANDARD OR SIGNAL POLE**



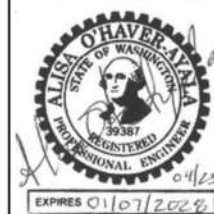
MOUNTING ON POST

STREET NAME SIGNS

- GREEN SIGN (9" X VARIES) WITH 6" WHITE TEXT
- FLAT BLADE
- HIGH INTENSITY PRISMATIC

NOTE
FOR MOUNTING ON SIGN POST
SEE DETAIL R6-15.2

APPROVED BY CITY ENGINEER

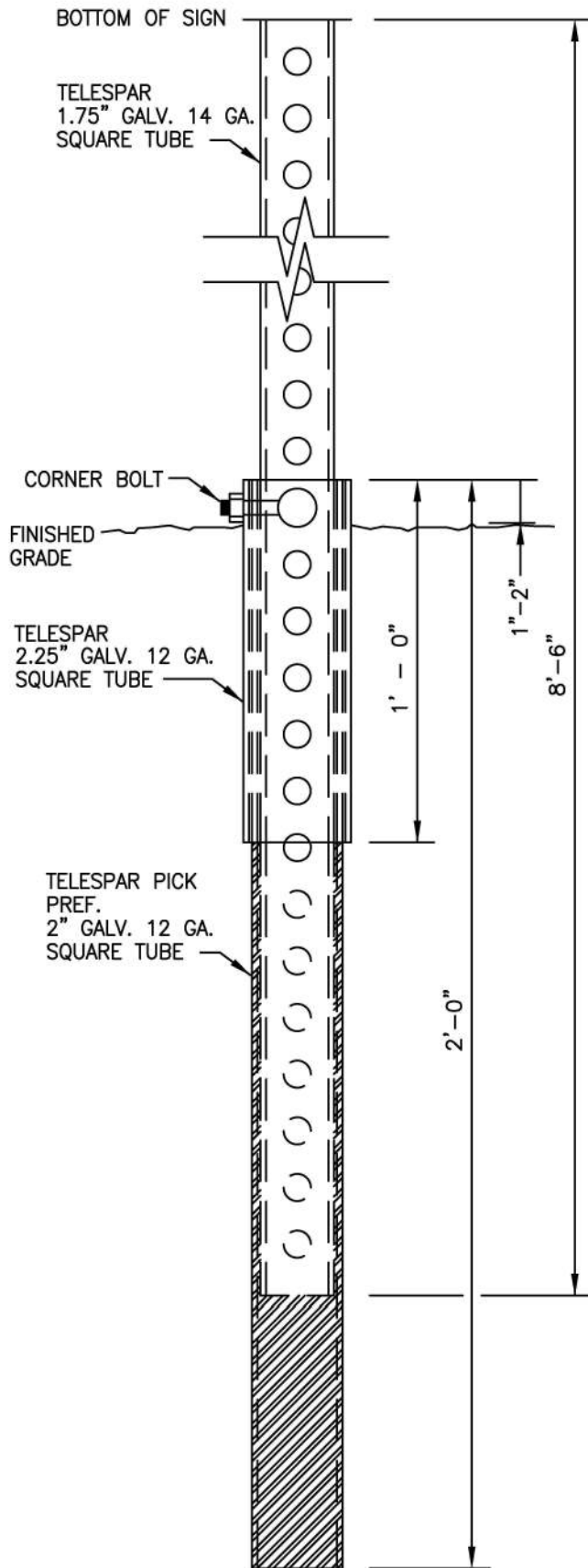


SIGN MOUNTING

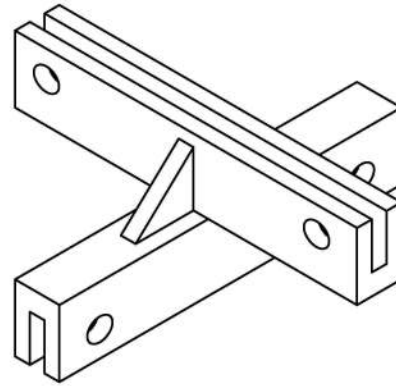
LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

STREET
STANDARD DETAIL
R6-15.1

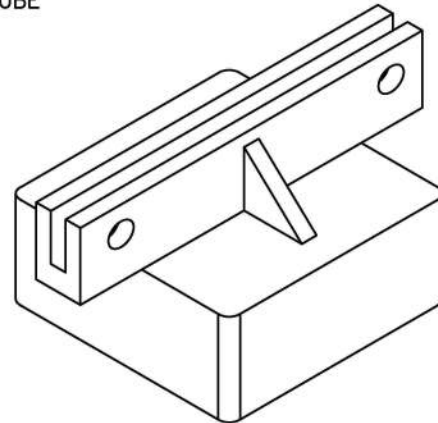


SQUARE TUBE SIGN POST



CROSSPIECE 808 X-F LONG CROSS
TOP FLAT 5.25 OR 6 INCHES LONG ALLOWED

8'-6"
TO BOTTOM OF SIGN
1.75" GALV. 14 GA.
SQUARE TUBE

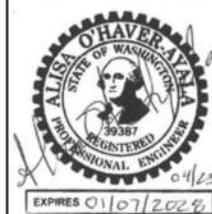


POST CAP 850 LONG
1.78FL BRACKET SQUARE CAP PLAT-LONG
5.25 OR 6 INCHES LONG ALLOWED

NOTES:

1. SIGN POSTS ARE THREE PIECE REQUIRING THE USE OF 2.25", 2" AND 1.75" SQUARE TUBE
2. CONCRETE SIGN BASE SHALL BE FINISHED SMOOTH AND LEVEL WITH FINISHED GRADE.

APPROVED BY CITY ENGINEER



SIGN POST AND CAP

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

STREET
STANDARD DETAIL
R6-15.2

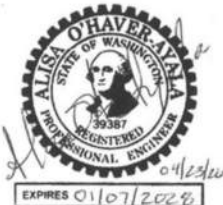



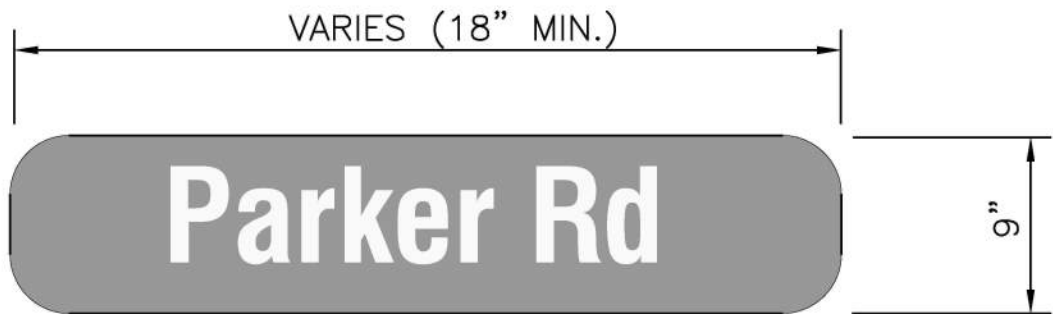
SIGN SIZE

VARIABLE (18" MIN.) x 8"
 FLAT BLADE STANDARD RADIUS, .08 ALUMINUM THINCKNESS

NOTES:

1. LETTERS: SERIES FHWA SERIES C FONT IDEAL, SERIES FHWA SERIES B ACCEPTABLE. WHITE (HIP) HIGH INTENSITY PRISMATIC.
2. INITIAL UPPER CASE LETTER FOLLWED BY LOWER CASE LETTERS.
3. SIGN BACKGROUND: DOUBLE SIDED, SOLID GREEN, NO BORDER REFLECTIVE, (HIP) HIGH INTENSITY PRISMATIC.
4. FOR MOUNTING ON SIGN POST SEE DETAIL R6-15.

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|---|---|
| APPROVED BY CITY ENGINEER  |  CITY OF SUMNER WASHINGTON |
| STREET NAME SIGNS FOR PRIVATE ROADS | |
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL |
| SHEET 1 of 1 | N.T.S. R6-16.1 |



LETTER SIZE



6" UPPERCASE LETTERS
 4" SUPPLEMENTAL LETTERING (St Ave Rd)

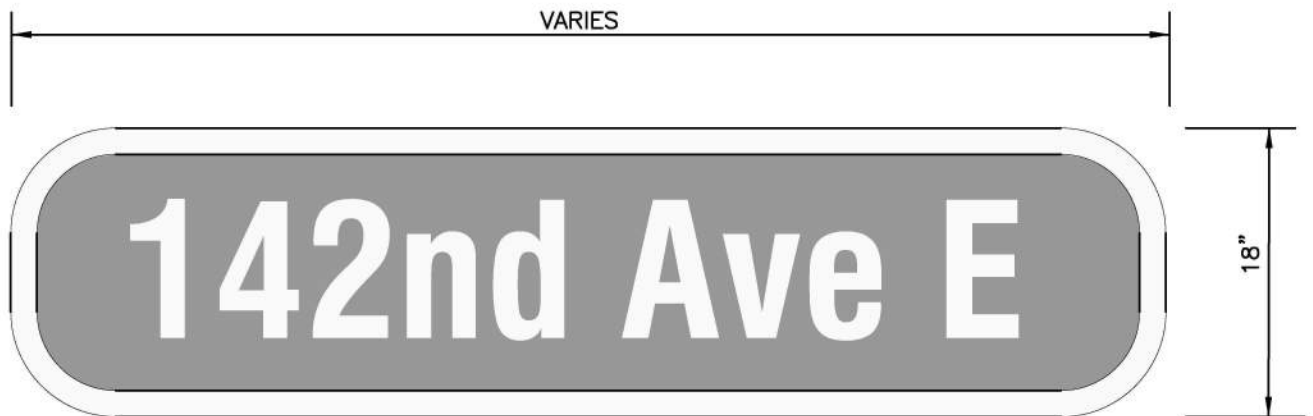
SIGN SIZE

VARIABLE (18" MIN.) x 9"
 FLAT BLADE STANDARD RADIUS,
 .08 ALUMINUM THICKNESS

NOTES:

1. LETTERS: SERIES FHWA SERIES C FONT IDEAL, SERIES FHWA SERIES B ACCEPTABLE. WHITE (HIP) HIGH INTENSITY PRISMATIC.
2. INITIAL UPPER CASE LETTER FOLLOWED BY LOWER CASE LETTERS.
3. SIGN BACKGROUND: DOUBLE SIDED, SOLID GREEN, NO BORDER REFLECTIVE, (HIP) HIGH INTENSITY PRISMATIC.
4. FOR MOUNTING ON SIGN POST SEE DETAIL R6-15.

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|--|---|--|
| APPROVED BY CITY ENGINEER |  | |
|  | STREET NAME SIGNS | |
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL R6-16.2 | |
| SHEET 1 of 1 | N.T.S. | |



LETTER SIZE



12" UPPERCASE LETTERS
 9" SUPPLEMENTAL LETTERING (St. Ave. Rd.)

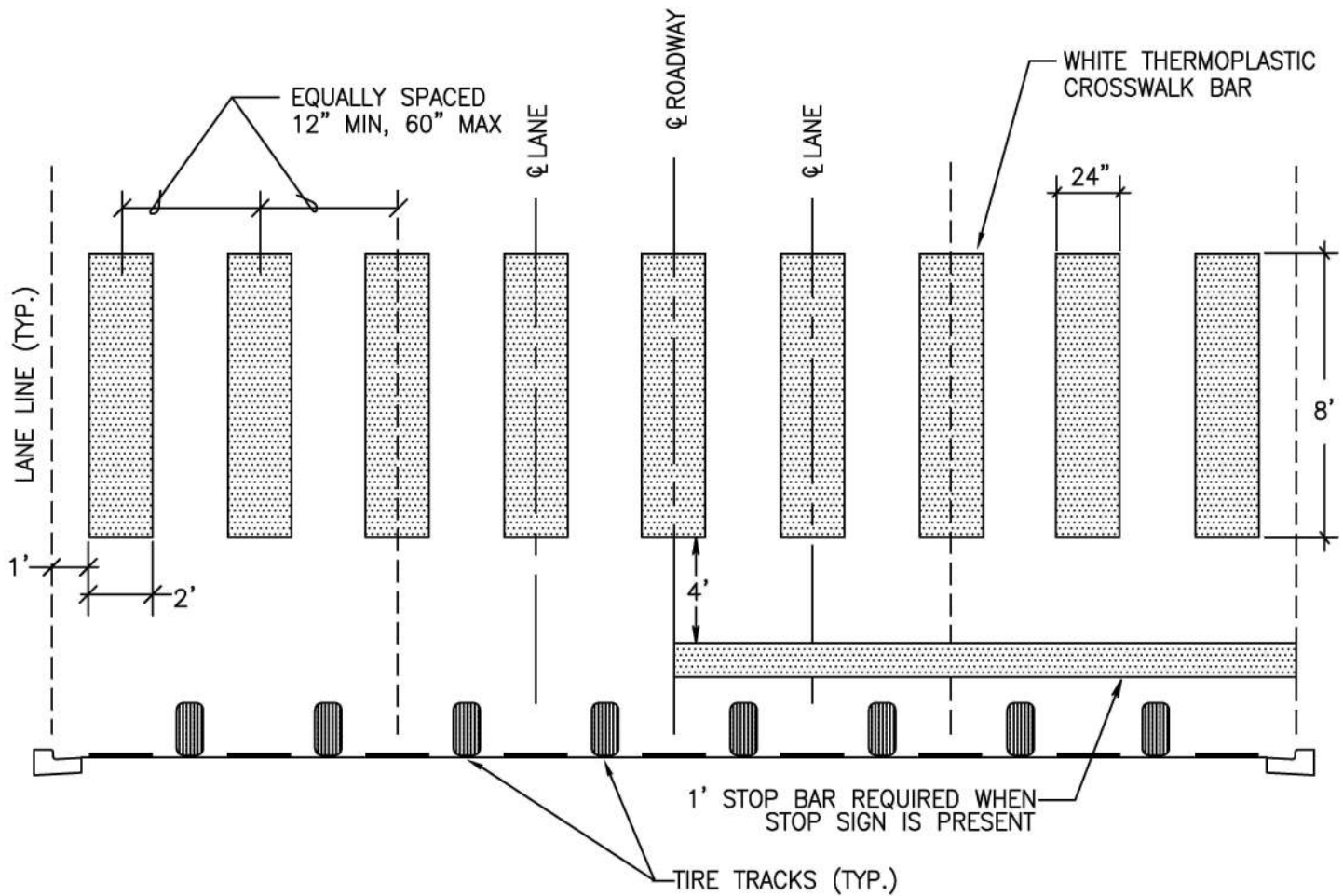
SIGN SIZE

VARIABLE x 18"
 FLAT BLADE STANDARD RADIUS,
 .125 ALUMINUM THICKNESS

NOTES:

1. LETTERS: SERIES FHWA SERIES C FONT IDEAL, SERIES FHWA SERIES B ACCEPTABLE. WHITE (HIP) HIGH INTENSITY PRISMATIC.
2. INITIAL UPPER CASE LETTER FOLLOWED BY LOWER CASE LETTERS.
3. SIGN BACKGROUND: DOUBLE SIDED, SOLID GREEN, WHITE BORDER ON OUTER EDGE, REFLECTIVE, (HIP) HIGH INTENSITY PRISMATIC.
4. FOR MOUNTING ON SIGN POST SEE DETAIL R6-15.

| | | |
|---|---|--------------------------------------|
| APPROVED BY CITY ENGINEER  |  | |
| | MAST ARM MOUNTED STREET NAME SIGNS | |
| LAST REVISION: 04/01/26 SHEET 1 of 1 | N.T.S. | STREET STANDARD DETAIL R6-16.3 |

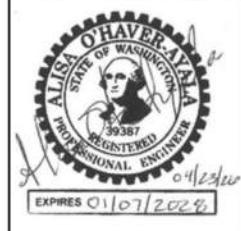


* TYPICAL 4 LANE ROADWAY CONFIGURATION

*** NOTE:**

FOR ROADWAYS WITH MORE OR LESS LANES. THE SAME CONFIGURATION APPLIES, KEEPING THERMOPLASTIC BARS CENTERED ON THE LANE LINES, AND IN THE CENTER OF THE TRAVELED PORTION OF THE LANE TO MINIMIZE TIRE WARE ON THE THERMOPLASTIC.

APPROVED BY CITY ENGINEER

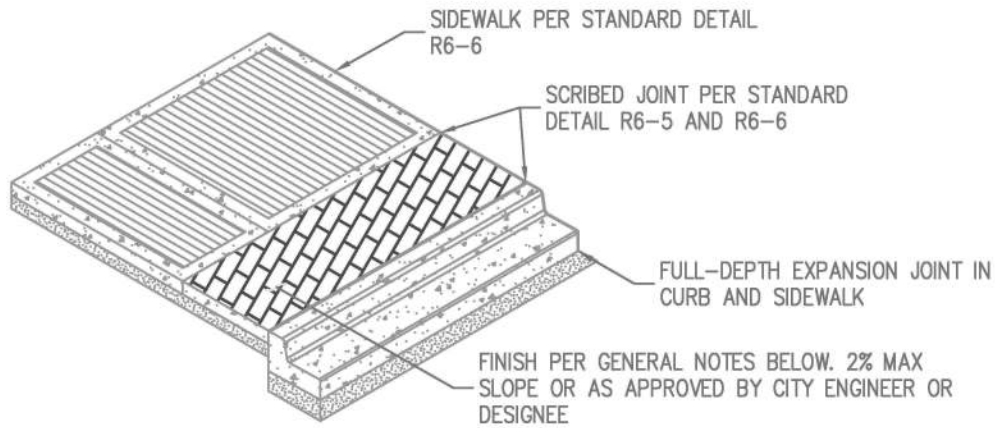


THERMOPLASTIC CROSSWALKS

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.



STREET
STANDARD DETAIL
R6-17

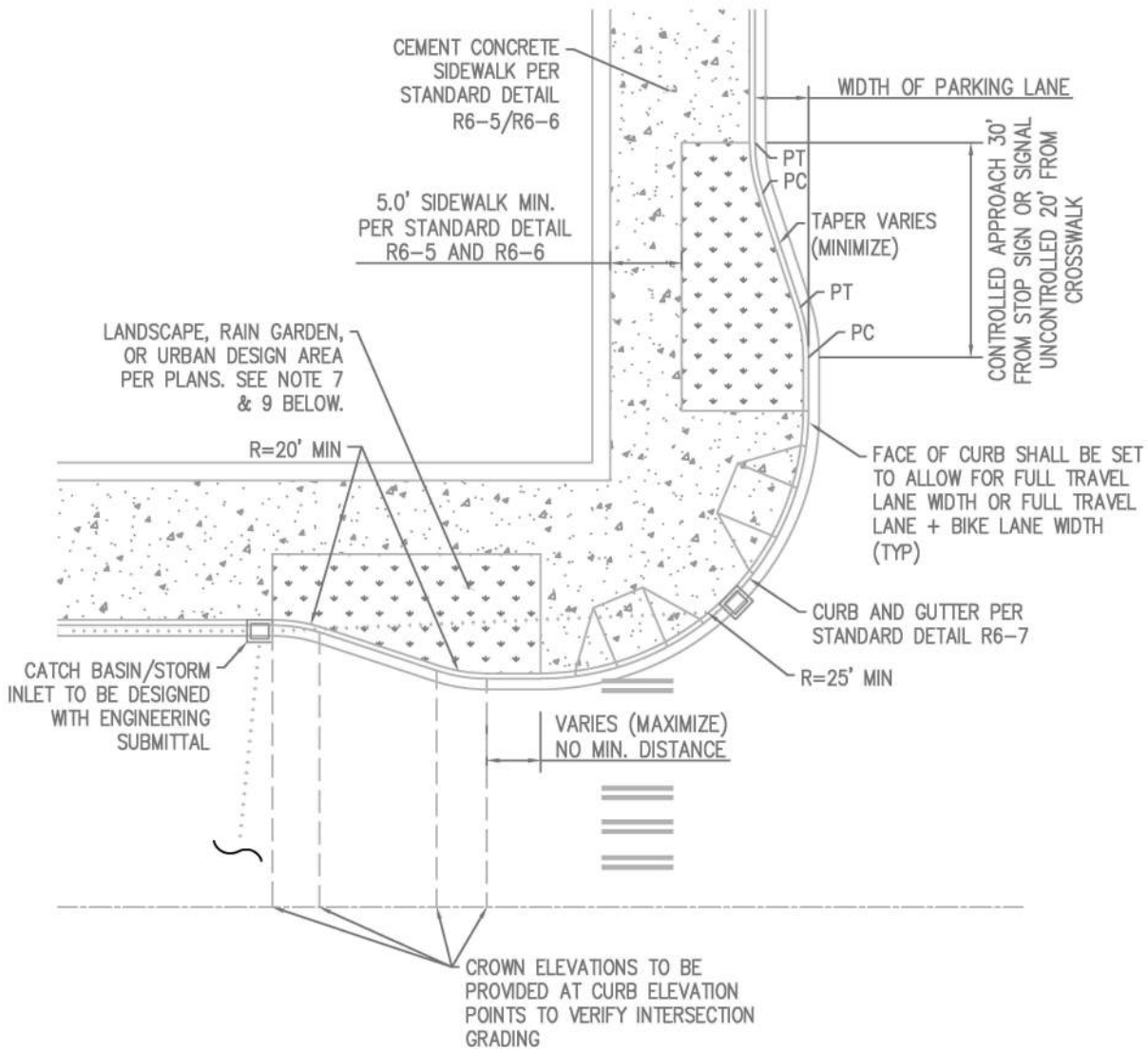


JOINT AND FINISH DETAIL
ISOMETRIC

GENERAL NOTES

1. STAMPED CONCRETE SHALL BE FINISHED WITH AN IMPRINT CONCRETE STAMP, POLYMER MAT. IMPRINTED CONCRETE PATTERN SHALL BE MADE FROM INTERLOCKING STAMP PATTERNS AS DETAILED ON THE PLANS. THE SURFACE TEXTURE IS THAT OF NEW, UNUSED BRICK, 3-5/8" WIDE BY 7-5/8" LONG, EDGES ARE STRAIGHT AND CORNERS ARE SQUARE, LAID IN A HERRINGBONE PATTERN. IMPRINTED INSIDE JOINTS HAVE THE APPEARANCE OF RAKED, ROUGH, SANDY GROUT JOINT, 3/8" WIDE. HERRINGBONE PATTERN SHALL BE LAID IN A 45° ANGLE TO THE CORNER RADIUS OR AS APPROVED BY THE CITY.
2. PRIOR TO START OF PAVEMENT WORK THE CONTRACTOR SHALL PROVIDE TWO 4'X4' (16 SQUARE FEET) STAMPED CONCRETE SAMPLES, AS DESCRIBED HEREIN. THE ENGINEER OR DESIGNEE SHALL THEN SELECT AND APPROVE THE ONE FINAL SAMPLE USED FOR STAMPED CONCRETE FINISH.
3. EACH 4'X4' SAMPLE AREA SHALL CONTAIN TWO VARIATIONS. SAMPLES SHALL VARY IN APPLICATION OF THE RELEASE AGENT AND THE ACCENT STONE STAINING, AS DIRECTED BY THE ENGINEER, AND SEALED WITH APPROVED SEALER TO DETERMINE THE FINAL VISUAL QUALITY OF THE IMPRINTED FINISH.
4. THE APPROVED SAMPLE SHALL BE THE STANDARD FOR ACCEPTANCE OF THE REST OF THE WORK INSTALLED, AND SHALL BE PROTECTED FROM DAMAGE UNTIL FINAL ACCEPTANCE AND APPROVAL. COMPLETED WORK NOT MEETING THE VISUAL QUALITY OF THE APPROVED SAMPLE SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
5. THE STAMPED CONCRETE FINISH SHALL RECEIVE A TWO STEP COLOR PROCESS, AS FOLLOWS:
 -COLOR HARDENER - COLOR "RED", SHALL BE A HIGH-OPACITY, UV RESISTANT, POWDERED DRYSHAKE COLOR HARDENER BROADCASTED ONTO FRESHLY LAID CONCRETE PAVEMENT PRIOR TO IMPRINTING WITH STAMP PATTERN.
 -COLOR RELEASE AGENT - COLOR "MAROON", SHALL BE A COLORED POWDER PROVIDING A NATURAL, WEATHERED ANTIQUE APPEARANCE THAT ACCENTS THE DEPTH OF THE PATTERN AND ADDS REALISTIC VARIATION TO IMPRINTED CONCRETE.
 -CLEAR SEALER -SHALL BE SOLVENT-BORNE, CLEAR MATTE FINISH, CLEAR CURING COMPOUND, PROTECTS THE CONCRETE SURFACE FROM FUTURE STAINING, RESISTANT TO BLUSHING, RESISTANT TO DISCOLORATION AND ULTRAVIOLET LIGHT. APPLY SEALER AT FULL STRENGTH PER MANUFACTURER'S RECOMMENDATIONS.
6. THE COLOR HARDENER AND RELEASE AGENT SHALL BE APPLIED EVENLY TO THE SURFACE OF FRESH CONCRETE, AND SEALER APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS. STAMPED CONCRETE FINISH AREAS SHALL BE ALLOWED TO CURE FOR A MINIMUM OF 28 DAYS PRIOR TO APPLICATION OF CONCRETE COLOR STAIN. PRESSURE WASH AREA FREE OF DUST, CONTAMINANTS AND DEBRIS AND ALLOW TO DRY PRIOR TO APPLICATION OF CONCRETE COLOR STAIN. APPLY STAIN ACCORDING TO MANUFACTURER'S RECOMMENDATIONS, AND TO ACHIEVE THE APPEARANCE OF THE APPROVED SAMPLE.
7. ALL STAMPED CONCRETE FINISH SHALL BE CURED AND SEALED WITH A WATERBORNE, LOW VOC, ENVIRONMENTALLY SOUND, CLEAR CURING COMPOUND AND SEALER FOR FRESHLY PLACED COLORED CONCRETE FLATWORK WITH COMPLIANCE TO ASTM C309. SEALER SHALL PROTECT AGAINST FUTURE STAINING, RESISTANT TO ABRASION, DEICING SALTS AND UV RADIATION.
8. DEPTH OF CONCRETE PANELS AND CONCRETE PANEL BASE SHALL BE PER CITY STANDARDS. IF NO STANDARD DETAIL EXISTS, APPLICANT SHALL SUBMIT DESIGN CALCULATIONS FOR PAVEMENT AND BASE DEPTHS.
9. DEPTH IN SIDEWALK LOCATIONS SHALL MATCH THE DEPTH OF THE SIDEWALK. FOR ROADWAY APPLICATIONS, DEVELOPER ENGINEER SHALL PROVIDE CONCRETE PAVEMENT DESIGN AND SUBMIT TO CITY FOR REVIEW.

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|---|--|
| APPROVED BY CITY ENGINEER  |  CITY OF SUMNER WASHINGTON |
| STAMPED CONCRETE | |
| LAST REVISION: 04/01/26 | STREET STANDARD DETAIL |
| SHEET 1 of 1 | N.T.S. R6-18 |



GENERAL NOTES

1. DEVELOPER ENGINEER TO PROVIDE ELEVATIONS AT ALL PC'S, PT'S, PI'S, LOW AND HIGH POINTS ALONG FLOW LINE WITH RECIPROCAL ELEVATIONS AT ROADWAY CROWN TO PROVIDE ABILITY TO REVIEW DRAINAGE AND CROSS SLOPE. DEVELOPER ENGINEER TO PROVIDE DRAINAGE AT ALL LOW POINTS.
2. CEMENT CONCRETE TRAFFIC CURB AND GUTTER SHALL BE PER STANDARD DETAIL R6-7.
3. CURB RAMP TYPE TO BE APPROVED BY THE CITY.
4. CURB RAMP CONFIGURATION PER WSDOT STANDARDS AND ADA CODES.
5. STORM DRAIN INLETS SHALL BE LOCATED OUTSIDE CURB RAMP LOCATIONS.
6. ALL LANDSCAPE AREAS IN CURB EXTENSIONS SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL M-11. LANDSCAPE PLANTERS WILL NOT BE ALLOWED INSIDE THE TOWN CENTER BOUNDARY.
7. CURB RADII VARY, SEE COS DEVELOPMENT STANDARD 6.1.5. CITY MAY REQUIRE A TURNING MOVEMENT DIAGRAM TO VERIFY DESIGN VEHICLE CLEARANCES ARE MET.
8. FOR ALL CURB EXTENSIONS WITH HARD SURFACE EXCEEDING 1,000 SQUARE FEET AS MEASURED FROM THE TERMINUS (PC-PT), THE CONTRACTOR SHALL USE A MINIMUM OF 20% COLORED AND/OR STAMPED CONCRETE. ALTERNATIVES TO STAMPED CONCRETE MAY BE USED IN AREAS WITH SLOPE GREATER THAN 2%.
9. THE DEVELOPER SHALL IDENTIFY THE PEDESTRIAN ACCESS ROUTE (PAR), WHICH SHALL BE DESIGNED TO MEET ALL APPLICABLE ADA REQUIREMENTS. AREAS LOCATED OUTSIDE OF THE PAR MAY EXCEED ADA SLOPE LIMITS; THESE AREAS SHALL BE CLEARLY IDENTIFIED ON THE PLANS AND SUBMITTED TO THE CITY ENGINEER OR DESIGNEE FOR REVIEW AND APPROVAL. NON-PAR AREAS SHALL BE CONSTRUCTED WITH EITHER A PLANTER STRIP OR STAMPED CONCRETE, AT THE DISCRETION OF THE CITY ENGINEER OR DESIGNEE.

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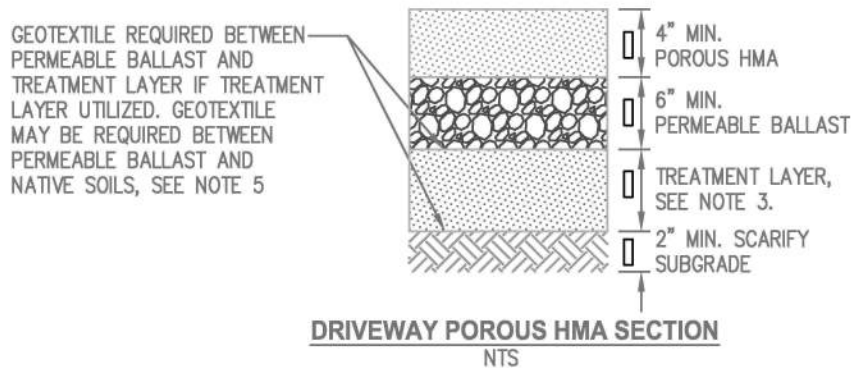
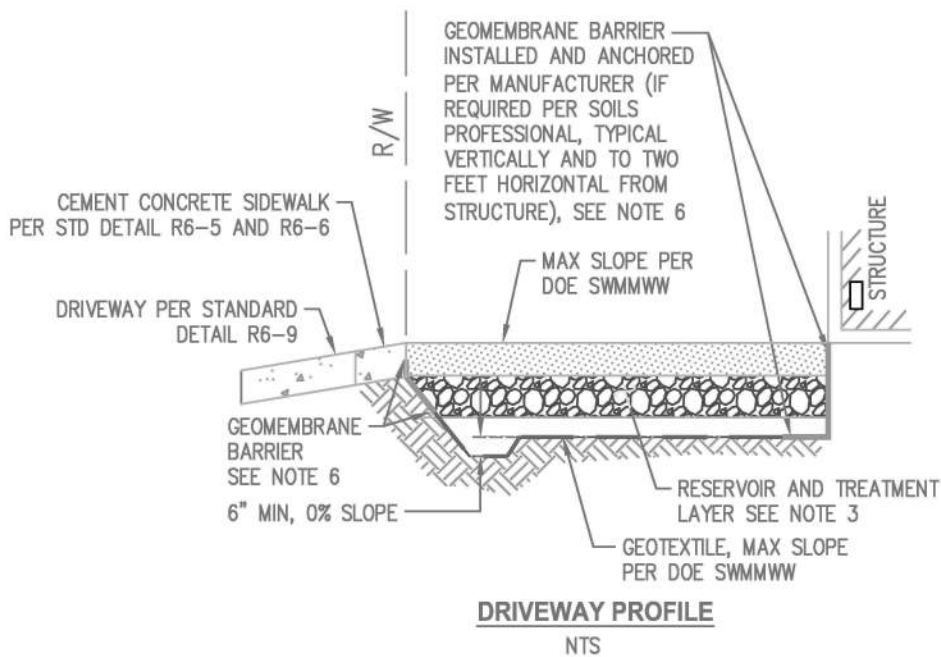


CURB EXTENSIONS

LAST REVISION: 04/01/26

STREET
STANDARD DETAIL
R6-19

SHEET 1 of 1 N.T.S.



GENERAL DRIVEWAY NOTES

1. PERMEABLE PAVERS MAY BE ALLOWED. CROSS SECTION SHALL MEET MANUFACTURER'S RECOMMENDATIONS AND INCLUDE AN APPROPRIATE RESERVOIR LAYER. SOIL SCARIFICATION OF SUBGRADE SHALL MEET THE REQUIREMENTS OF THE DOE SWMMWW. ENGINEER TO PROVIDE CONTINUOUS RUNOFF MODELING CAPACITY CALCULATIONS.
2. LIMIT RUN-ON TO PERMEABLE PAVEMENT SURFACES TO THE MAXIMUM EXTENT FEASIBLE.
3. 6-INCH MINIMUM TREATMENT LAYER REQUIRED FOR ALL PERMEABLE PAVEMENTS UNLESS EXISTING SOILS HAVE SHOWN TO PROVIDE THE REQUIRED CEC/ORGANICS TREATMENT CRITERIA AND INFILTRATION RATE PER DOE SWMMWW.
4. GEOTEXTILE LINER MAY BE REQUIRED BETWEEN NATIVE SOILS AND PERMEABLE PAVEMENT SECTION, PER GEOTECHNICAL ENGINEER. GEOTEXTILE WILL BE REQUIRED BETWEEN PERMEABLE BALLAST AND SAND LAYER. GEOTEXTILE SHALL BE GEOTEXTILE FOR UNDERGROUND DRAINAGE PER WSDOT STANDARD SPECIFICATION 9-33.2(1), NONWOVEN, TABLE 1 AND INSTALLED PER WSDOT STANDARD SPECIFICATION 2-12.3(1).
5. PERMEABLE PAVEMENT SURFACING SHALL MEET APWA GSP 5-04.3 CONSTRUCTION REQUIREMENTS POROUS ASPHALT (PHMA/PWMA) ACCEPTANCE INFILTRATION TEST FOR POROUS ASPHALT.
6. GEOMEMBRANE BARRIER SHALL PROVIDE AN IMPERMEABLE BARRIER BETWEEN STANDARD AND PERMEABLE SECTION. IT SHALL BE INSTALLED BELOW FINISHED GRADE. GEOMEMBRANE BARRIER SEAMS SHALL OVERLAP AT LEAST 18" OR PER MANUFACTURER'S RECOMMENDATIONS. GEOMEMBRANE BARRIER SHALL EXTEND THE LENGTH OF THE PERMEABLE SECTION WHEN ADJACENT TO DENSE PAVEMENT.
7. THIS FIGURE APPLICABLE TO DRIVEWAYS THAT ARE FLAT OR SLOPE AWAY FROM THE STRUCTURE. ADDITIONAL DESIGN CONSIDERATIONS ARE REQUIRED WHEN THE DRIVEWAY SLOPES TOWARD THE STRUCTURE.

APPROVED BY CITY ENGINEER

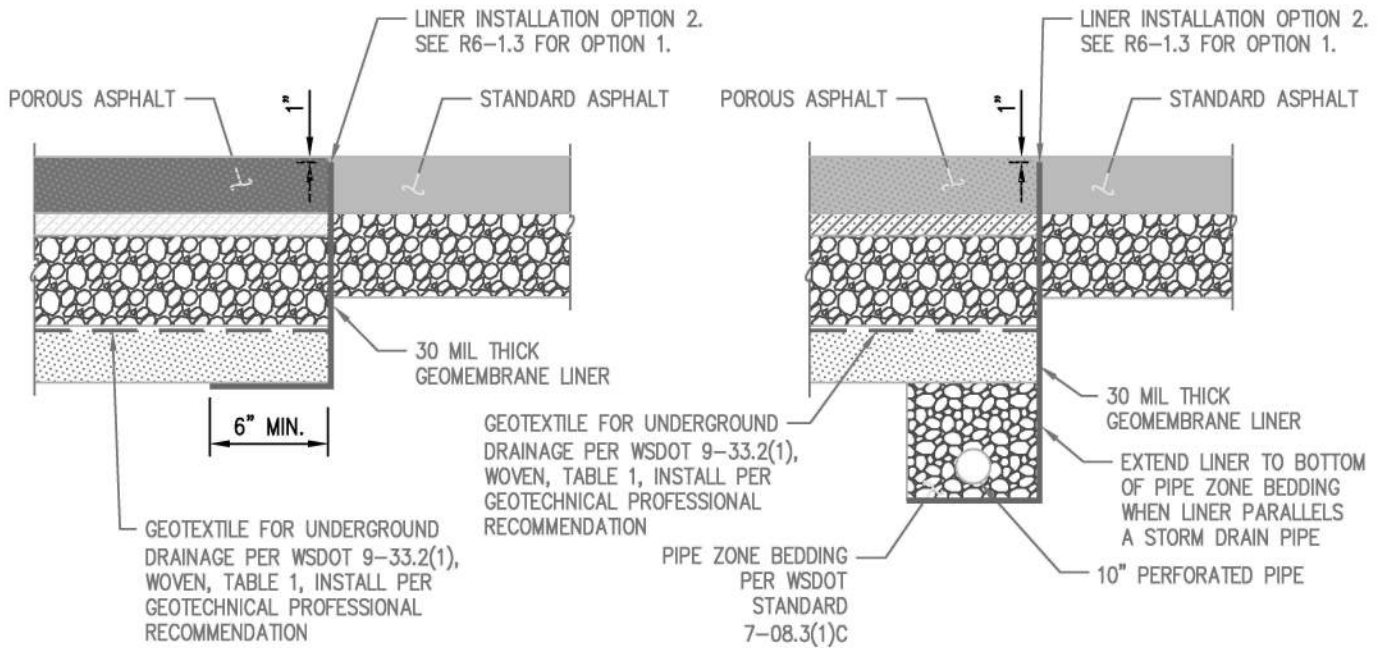


PERMEABLE DRIVEWAY

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

STREET
STANDARD DETAIL
R6-21.1

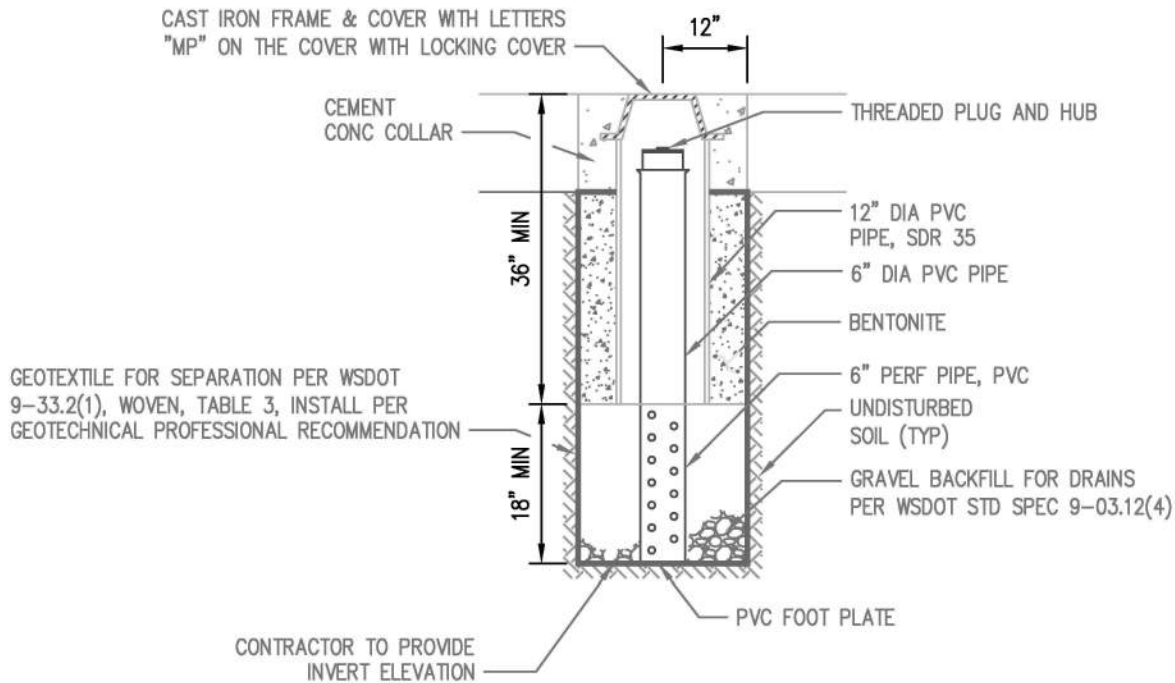


LINER WITHOUT UNDERDRAIN

LINER ALONG UNDERDRAIN

NOTES:

A DEEPER SECTION OF PERMEABLE BALLAST CAN BE USED IN LIEU OF AN UNDERDRAIN PIPE AT THE DOWN GRADIENT END AS APPROVED BY THE CITY ENGINEER OR DESIGNEE. MINIMUM ADDITIONAL PERMEABLE BALLAST MEASURING 6 INCH DEPTH, 2- FEET ALONG THE RUNNING SLOPE OVER THE FULL WIDTH OF PERVIOUS PAVEMENT.



MONITORING PORT

NOTES:

A MINIMUM OF ONE INSPECTION PORT IS REQUIRED PER SYSTEM. AN ADDITIONAL INSPECTION PORT IS REQUIRED FOR EVERY 5,000 SQUARE FEET OF PERMEABLE PAVEMENT. INSPECTION PORTS SHALL BE LOCATED A MINIMUM OF THREE (3) FEET FROM THE EDGE OF PAVEMENT, AND BE LOCATED AT THE LOW POINT OF THE SYSTEM. EXCEPTION: THIS REQUIREMENT DOES NOT EXTEND TO SIDEWALKS AND NON-TRAFFIC BEARING SURFACES.

APPROVED BY CITY ENGINEER



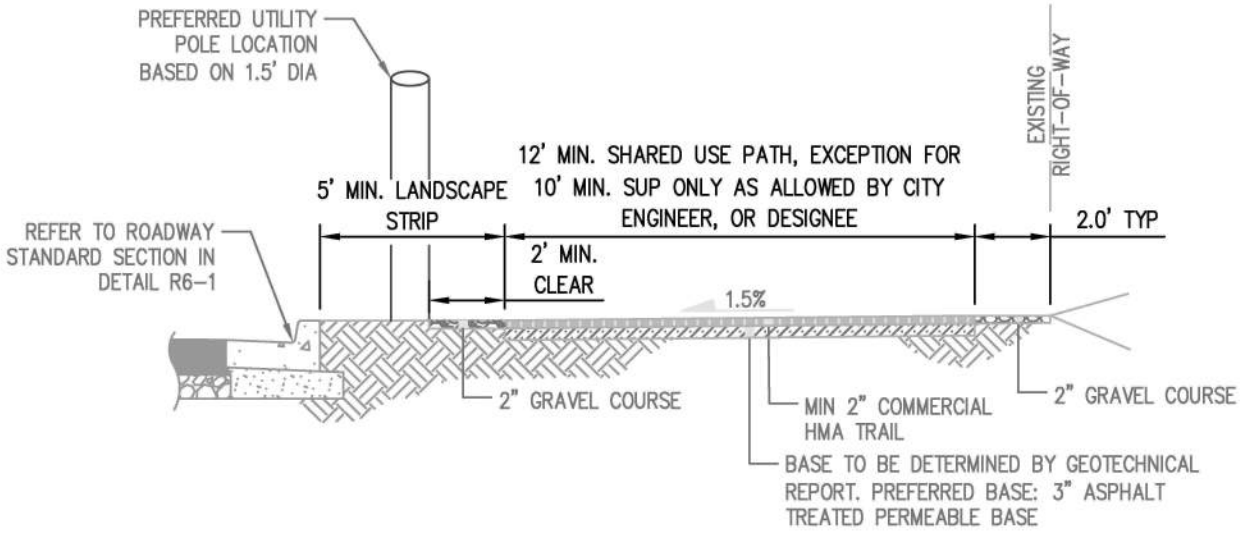
PERMEABLE PAVEMENT DETAILS

LAST REVISION: 04/01/26

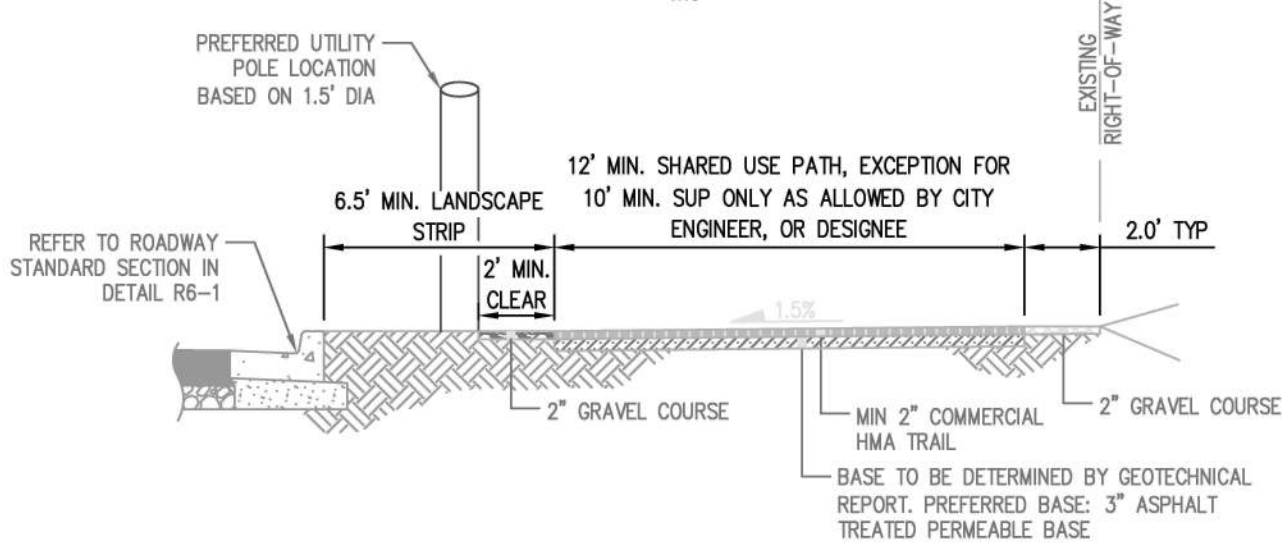
STREET STANDARD DETAIL R6-21.3

SHEET 1 of 1

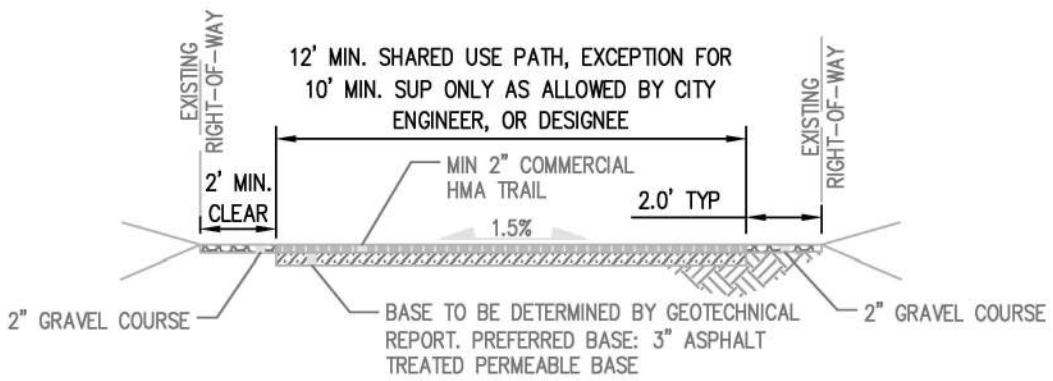
N.T.S.



SHARED USE PATH 30 MPH AND LESS
NTS



SHARED USE PATH 35 MPH AND GREATER
NTS



SHARED USE PATH UNCONNECTED FROM ROADWAY
NTS

GENERAL NOTES

1. WHEN WITHIN 200' OF WHITE RIVER, TRAIL SHALL BE PERMEABLE AND HAVE 5' PLANTER STRIPS WITH STREET TREES (REFERENCE DETAIL R6-13) ON EITHER SIDE OF TRAIL.
2. SHARED USE PATH ON ELM ST AND WOOD AVE NORTH OF ZEHNDER SHALL BE CONCRETE INSTEAD OF ASPHALT.

APPROVED BY CITY ENGINEER

ALISA O'HAVER
STATE OF WASHINGTON
59387
REGISTERED PROFESSIONAL ENGINEER
EXPIRES 01/07/2028



SHARED USE PATH SECTION

LAST REVISION: 04/01/26

SHEET 1 of 1 N.T.S.

STREET STANDARD DETAIL
R6-22